



POSSIBLE INFRASTRUCTURE STRATEGIES FOR BRISTOL

Overview

A Plan of Conservation and Development (POCD) lays out the long-term vision for the physical, economic, and social development of a community. This booklet is intended to promote discussion about the services and facilities the City might want to have to promote its overall conservation and development goals and promote the public health, safety, and welfare (“infrastructure strategies”).

The other themes being addressed as part of the updating of the 2000 POCD include:

- **Conservation Strategies** - what the City might consider preserving, protecting, or enhancing in order to promote the public health, safety, and welfare.
- **Development Strategies** – how the City might want to guide growth and change in order to promote the public health, safety, and welfare.

“The form a city assumes as it evolves over time owes more to ... what we now call infrastructure than almost any other factor ...”

Martin Filler
American architecture critic

Community Facility



Community Facility



Roadway Circulation



Pedestrian Circulation



Transit Service



Utilities



Possible Community Facility Strategies

Overview (adapted from 2000 POCD)

As a community grows and its population changes, as existing facilities become outmoded, and as public expectations change, the demand for additional, improved and varied public facilities [and services] also changes.

The nature, extent and condition of a community's [facilities and services] all contribute significantly to its quality of life. This chapter summarizes the major components of the public [facilities and services] in Bristol that directly affect – and/or are affected by – land use and development policies and decisions.

The emphasis of this chapter is on long-term land use and capital planning issues, not the day-to-day management or operations of individual municipal departments.

Key Findings, Conclusions and Issues

City Hall

- City Hall was originally built in 1963 (50 years ago) and renovated in 1983 (30 years ago)
- The *Space Needs Analysis* (2012) conducted for the City indicated that the building is in good structural condition but that many of the building systems have met their useful life expectancy.
- Building renovation will require upgrading to address a number of code issues
- Some City departments are located away from City Hall due to a lack of space at the facility
- The *Space Needs Analysis* suggested the possibility of re-using Memorial Boulevard School for City Hall

City Hall



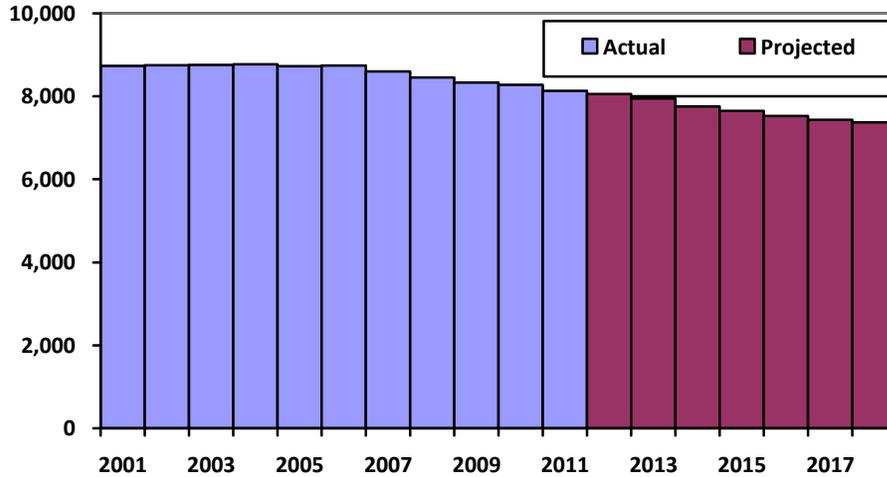
Memorial Boulevard School



Educational Facilities

- Total public school enrollment in Bristol has been decreasing since 2006.
- Declining enrollments in Bristol are a reflection of the overall economy, an aging population, and age-related migration patterns.
- Overall public school enrollment is projected to decrease for the foreseeable future.

Total K-12 Enrollment Projections



- The City has been reconfiguring schools to:
 - Relate available capacity to projected enrollment
 - Balance diversity within parameters established by the State
 - Utilize the best facilities for educational programs
- As a result, the City has:
 - Closed several elementary schools (O'Connell, Bingham, Jennings)
 - Closed one middle school (Memorial Boulevard)
 - Built two new K-8 schools (West Bristol and Forestville/Greene-Hills)
- Since enrollment is projected to decline within every grade group (PK-5, 6-8, and 9-12), school capacity is not expected to be an issue during the planning period.

Public Safety Facilities and Services

- The Police Department Building was built in 1978 (35 years ago)
- The *Space Needs Analysis* (2012) conducted for the City indicated that space in the police building for the Police Department is “very cramped and in much need of expansion.”
- However, the ability to expand within this building is limited due to the presence of the Superior Court facility.
- The general recommendation is that one of the uses in this building will need to be relocated in order to have enough space for anticipated police department needs during the planning period.

- Bristol has five fire stations and a full-time fire department
- Parts of the southwestern sector of Bristol are the most distant from the existing fire stations
- In terms of the main fire station on North Main Street, the *Space Needs Analysis* conducted for the City indicated the current building is undersized for the needs of the Fire Department and parking is tight
- The report recommended expanding the facility by adding an additional bay (complicating the parking situation) or relocating the facility

- Emergency medical response in Bristol is provided by Bristol Hospital EMS, a fully licensed Paramedic service operated by an independent company
- This arrangement is expected to be adequate for the planning period and beyond

Police Station



Fire Headquarters



Public Works Garage

- Operations at the public works garage on Vincent P. Kelly Road can be inefficient due to a lack of service bays, storage space, and operational space.
- Strategies to address the long-term space needs at this facility should also be investigated.
- While not centrally located, the public works facility has good road access to all parts of the community
- In terms of waste disposal, the City has long term contracts in place for disposal of solid waste and for recycling
- During the planning period, Bristol will be participating with other communities to establish a composting facility to remove organic waste from the waste stream

Public Works Facility



Public Works Facility



Library Facilities

- Bristol's main library was expanded in 2007 and additional parking was added.
- This facility is expected to be adequate for community needs during the planning period.
- A satellite facility in Forestville (the Manross Memorial Library) is also expected to be adequate for community needs during the planning period.

**Bristol Public Library
High Street**



**Manross Memorial Library
Central Street**



Park And Recreation Facilities

- The City's Parks and Recreation Department is responsible for the City's parks, playgrounds and open space
- In conjunction with facilities available at the City's public school sites, these areas help meet the active and passive recreational desires of Bristol residents.
- These City-owned sites are supplemented by private school facilities, State lands, non-profit organizations, and private recreational facilities (such as golf clubs)
- Since 2004, after a number of years of declining attention, the City has been implementing plans to improve City park facilities
- There is considerable pressure placed on recreation fields due to:
 - High local involvement in sports
 - Limitations arising from the need to apply herbicides, pesticides, etc.
 - Lack of lighting
 - "Season creep" where sports have longer seasons or secondary seasons
- To meet local needs, the Parks and Recreation Department would like to:
 - Establish multi-use synthetic fields
 - Light fields for evening use
- The southwest, northeast and northwest sections of Bristol lack formal neighborhood parks.
- The Parks and Recreation Department would like a community center facility which would allow them to offer indoor programming without conflicts with the School Department or other agencies

Senior Center

- The Beals Senior / Community Center on Stafford Avenue is expected to be adequate for community needs during the planning period.
- There is excess space at this facility which could be used to meet other community needs.

Surplus Facilities

- Jennings School is no longer being used by the City and the property is being repurposed for housing and a day care center.
- Memorial Boulevard School is no longer being used by the City and potential uses for the property are being evaluated by a Study Committee set up by the City Council
- O'Connell School is no longer being used by the City and potential uses for the property are being evaluated.
- Bingham School is no longer being used by the City and potential uses for the property are being evaluated.

Jennings School



Memorial Boulevard School



O'Connell School



Bingham School



GOAL

Continue to **efficiently** provide municipal **facilities and** services to best serve the needs and expectation of Bristol residents **and businesses**.

Policies:

1. Carefully consider the City's financial resources in the planning for future municipal facilities and services.
2. Ensure that adequate financial and staff resources are allocated for the maintenance of City facilities including buildings schools, parks, and recreational facilities in order to:
 - a. enrich the public's enjoyment of them
 - b. prevent physical deterioration
 - c. ensure their long-term cost effectiveness
3. Anticipate the need to upgrade and improve City facilities, as necessary, to keep pace with community needs.
4. Encourage the shared use of municipal facilities and educational facilities (during off hours) in order to make more efficient use of such facilities.
5. Establish a dedicated fund for the acquisition of land for parks, recreational facilities so that opportunities for acquisition can be acted upon expeditiously and/or such funds can be readily available to match State or federal funding programs.

GOAL

Establish a comprehensive process for acquiring or disposing of property.

Policies:

6. When property adjacent to a City facility becomes available, evaluate the desirability of acquiring that property for possible future expansion of such facilities.
7. Establish and use a comprehensive process whereby City departments and the overall community have opportunities to provide input regarding desirable future uses for surplus City buildings or properties.
 - a. Retain City-owned properties (such as schools) rather than selling them if the City determines that there is likely to be a future need or an alternative municipal use for the property.
 - b. Consider allowing or establishing playgrounds, playfields or passive park facilities (or other facilities which might benefit the neighborhood) on unused City-owned parcels as they become available.
 - c. Where appropriate, repurpose unused or excess land owned by the City (other than properties that fulfill important public purposes such as public parks or open space preserves) to productive use or to the City's tax rolls.

GOAL

Address identified municipal facility and service needs.

Policies:

8. Address the space / building needs at City Hall while maintaining a City Hall presence in downtown Bristol.
9. Address space needs within the Police Department.
10. Address space needs at the Fire Department Headquarters.
11. Monitor fire response times in the southwest corner of Bristol to help determine whether a satellite fire station might be warranted.
12. Ensure that adequate emergency medical response is maintained.
13. Investigate strategies to address the long-term space needs at the Public Works facility.
14. Continue to improve City park and recreation facilities to meet community needs.
15. Establish additional neighborhood recreational facilities in the northeast, northwest and southwest sections of Bristol appropriate to the specific needs of each area.
16. Improve existing recreational programs and expand coordination with semi-public and private recreational facilities.

GOAL

Provide a broad range of human services and educational opportunities to those Bristol residents who desire or need them.

Policies:

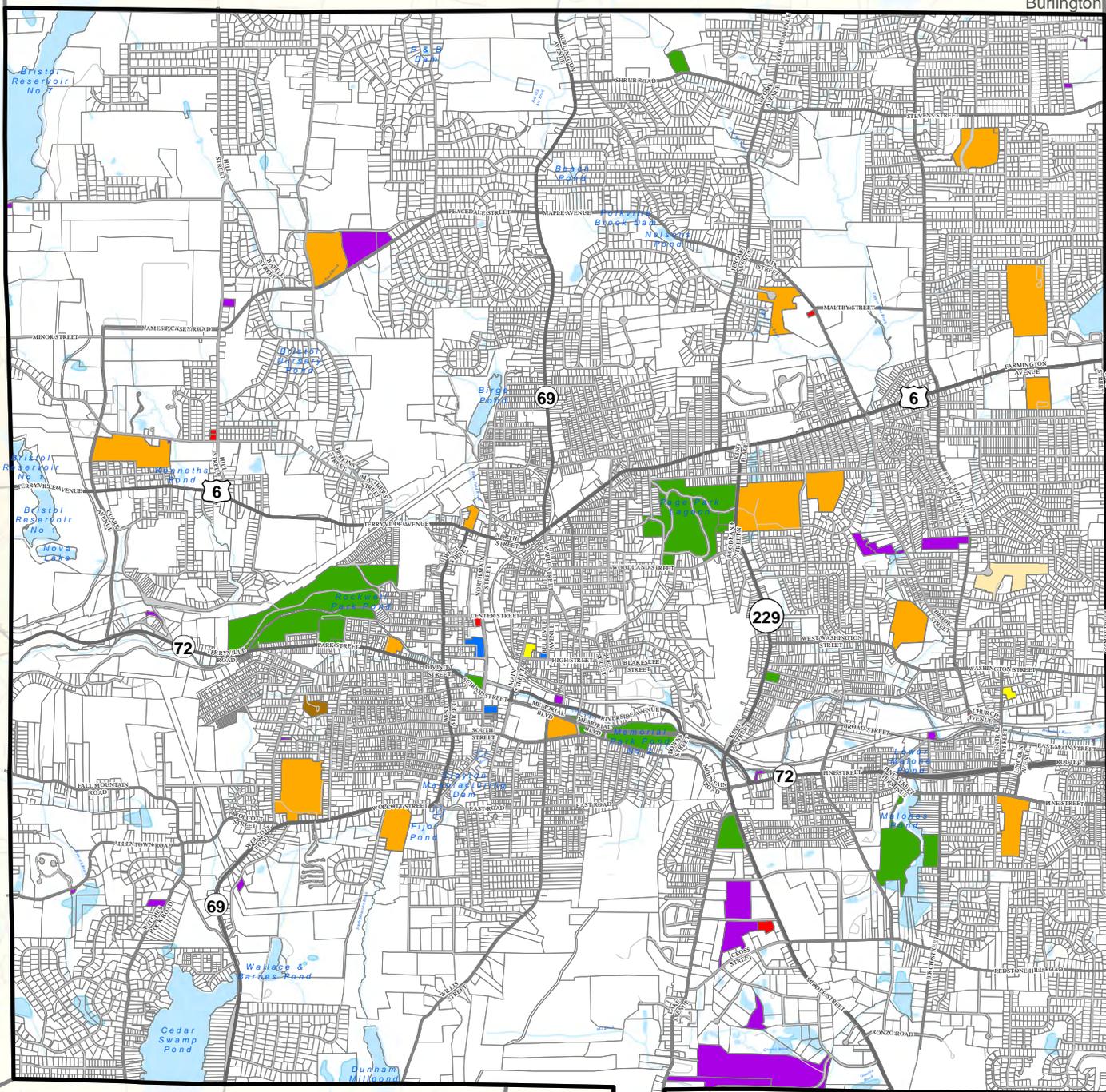
17. Provide a range of educational, recreational and social facilities consistent with the population to be served and at reasonable cost; locate such facilities so as to be easily accessible to residents of all ages.
18. Promote the provision of services such as child and adult day care for those persons who need such services to allow them to enter and remain in the job market.
19. Recognize the need for public and private pre-school education accessible to those who require such services for their children.
20. Allow for public and private educational facilities in appropriate locations.
21. Encourage the provision of adult education, to expand/upgrade general knowledge, vocational skills and cultural endeavors, at times and in locations that are convenient for potential enrollees.
22. Recognize the need for programs and facilities that address the needs of the homeless population.
23. Recognize the need for services and programs for the City's special needs population.

Municipal Facilities

Bristol, CT

DRAFT

Burlington



Wolcott

Southington

Legend

- | | |
|--|---|
|  Education |  Parking |
|  General Government |  Public Safety |
|  Housing |  Public Works |
|  Library |  Senior Center |
|  Park / Recreation | |



2,700 Feet

Possible Transportation Strategies

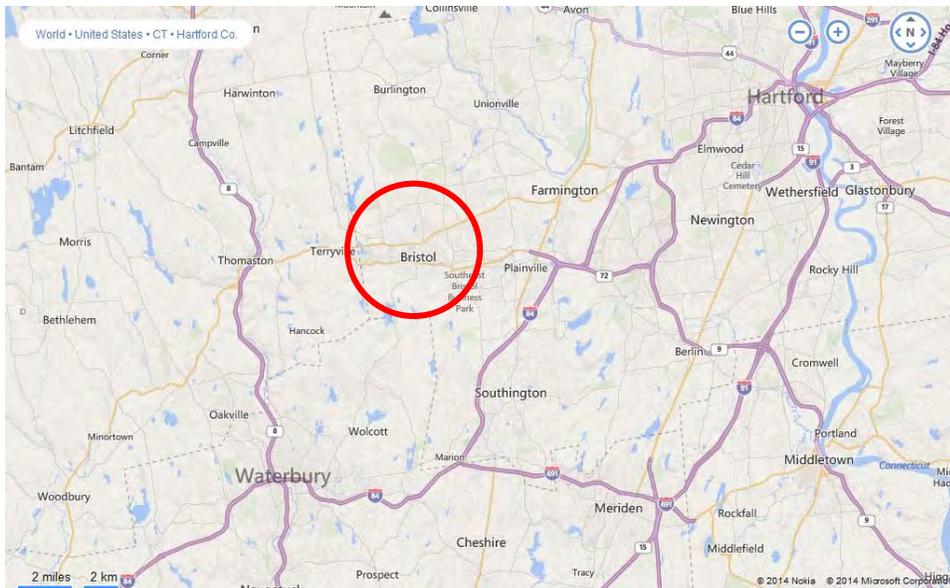
Overview (adapted from 2000 POCD)

At its most fundamental, the purpose of a community's transportation system is to move people and goods. It provides the primary means by which people can get to work, go shopping or spend their leisure time. It also provides the framework upon which the community is built and, as such, is inextricably bound to the land uses it serves. The number, quality and variety of transportation options in a community influence locational choices for the development of housing, businesses and industry.

This chapter briefly describes Bristol's transportation system, which is composed of City streets and State roads, public transit, sidewalks and bicycle paths. The chapter seeks to identify and address a number of existing problems in Bristol's transportation network as well as its future transportation needs.

Key Findings, Conclusions and Issues

- From a regional perspective, the most significant transportation issue facing the city is its lack of direct access to and from an interstate highway.
- While the recent improvement of Route 72 east of Route 229 has improved the situation, accessibility to Bristol is still perceived as an issue
- Cities with direct access to and from an interstate highway are at an advantage in terms of regional competition in the marketplace; such cities can offer critical access advantages to potential businesses and industries.
- As such, Bristol's lack of interstate access has implications for its economic development potential.



Vehicular Circulation / Roadways

- Bristol's street system is traversed by four state roads – Routes 6, 72, 69 and 229 – which play an important role in carrying local and area-wide traffic into, through and out of the community.
 - Routes 6 and 72 carry traffic in an east-west direction and link Bristol with Farmington, Plainville and Plymouth.
 - Routes 229 and 69 carry traffic in a north-south direction and link Bristol with Southington, Wolcott and Burlington.
- These state roads form the backbone of Bristol's roadway network and are its primary transportation connection to the larger region.
- Within the City limits, the most significant transportation issue is traffic congestion along Route 6 east of downtown (Farmington Avenue)
- This portion of the Route 6 corridor currently operates over capacity, and traffic flow is further impeded by:
 - multiple curb cuts and turning movements into and out of adjacent retail and residential land uses,
 - a lack of turning lanes at some locations
 - changing lane configurations where the roadway necks down from two travel lanes to one travel lane in some locations
 - signal spacing or the lack of signal timing/coordination
 - Even though the State plans to widen Route 6 east of Mix Street, the City needs to continue to utilize local land use and traffic management controls – such as curb cut consolidation, the synchronization of traffic signals and the realignment of intersections – to improve traffic mobility and safety along the corridor.
- The Connecticut Department of Transportation identifies locations on State highways where the accident experience is significantly different than average and there are a number of such locations in Bristol where the State is monitoring accident experience
- The City monitors the condition of pavement on City streets in order to identify and prioritize street maintenance needs
- Research has found that using a pavement management program to identify road maintenance needs before they become critical (and expensive) helps to stretch maintenance budgets and maintain roadways efficiently and cost effectively
- Pavement widths on local street can be wider than is necessary to accommodate the roadway function
- Roadways which are wider than necessary can encourage speeding, generate more runoff, and increase maintenance expenditures

- "Functional classification" (or the categorization of roads into different operational systems) groups streets and highways according to the **level of service** they are intended to provide.

Major arterials primarily function as routes for through traffic and for traffic movement between Bristol and surrounding communities:

1. **Route 6** – Farmington Avenue / North Street / Terryville Avenue
2. **Route 72** – East Main Street / Broad Street / Riverside Avenue / Main Street / School Street / Park Street / Terryville Road
3. **Route 229** – King Street / Middle Street
4. **Route 69** – Burlington Avenue / West Street / Wolcott Street (east of Wolcott Road) / Wolcott Road

Minor arterials (arterial, commercial, industrial) provide some through service and may connect major arterials but traffic volumes are lighter than along major arterials:

1. Allentown Road
2. Birch Street
3. Central Street
4. Church Avenue
5. Clark Avenue
6. Emmett Street (north of Redstone Hill Road)
7. Fall Mountain Road
8. James P. Casey Road-Peacedale Street-Maple Avenue-Mix Street
9. Jerome Avenue
10. Lake Avenue
11. Lincoln Avenue
12. Maltby Street
13. Memorial Boulevard
14. Pine Street-Mountain Road-South Street
15. Redstone Hill Road
16. Shrub Road
17. Stafford Avenue
18. Stevens Street
19. West Washington Street-Washington Street
20. Willis Street
21. Witches Rock Road
22. Wolcott Street (west of Wolcott Road)

Collectors provide for traffic movement from arterial roads to neighborhoods within Bristol and between neighborhoods:

1. Bellevue Avenue
2. Blakeslee Street
3. Brook Street
4. Center Street (west of Maple Street)
5. Church Street
6. Cross Street
7. Divinity Street
8. Downs Street
9. East Road
10. Hart Street
11. High Street
12. Hill Street
13. Lewis Street-Lewis Road (pt.)-Oakland Street (north of Farmington Avenue)
14. Louisiana Avenue (between Route 229 and Brook Street)
15. Main Street
16. Maple Street
17. Matthews Street
18. Minor Street
19. North Main Street
20. Oakland Street (between Farmington Avenue and Woodland Street)
21. Peck Lane
22. Perkins Street
23. Queen Street
24. Round Hill Road
25. Sonstrom Road
26. Todd Street
27. Woodland Street

Local streets provide direct access to the properties located along them.

All streets in Bristol not classified as major arterials, minor arterials or collectors are classified as local streets.

- According to City Ordinance 21-130, “all streets shall be classified as local residential, collector or arterial/commercial/ industrial dependent upon the proposed usage and the requirements of the zoning regulations and the plan of conservation and development”
- Based on these classifications, the layout geometry for the different classes of streets is as indicated below:

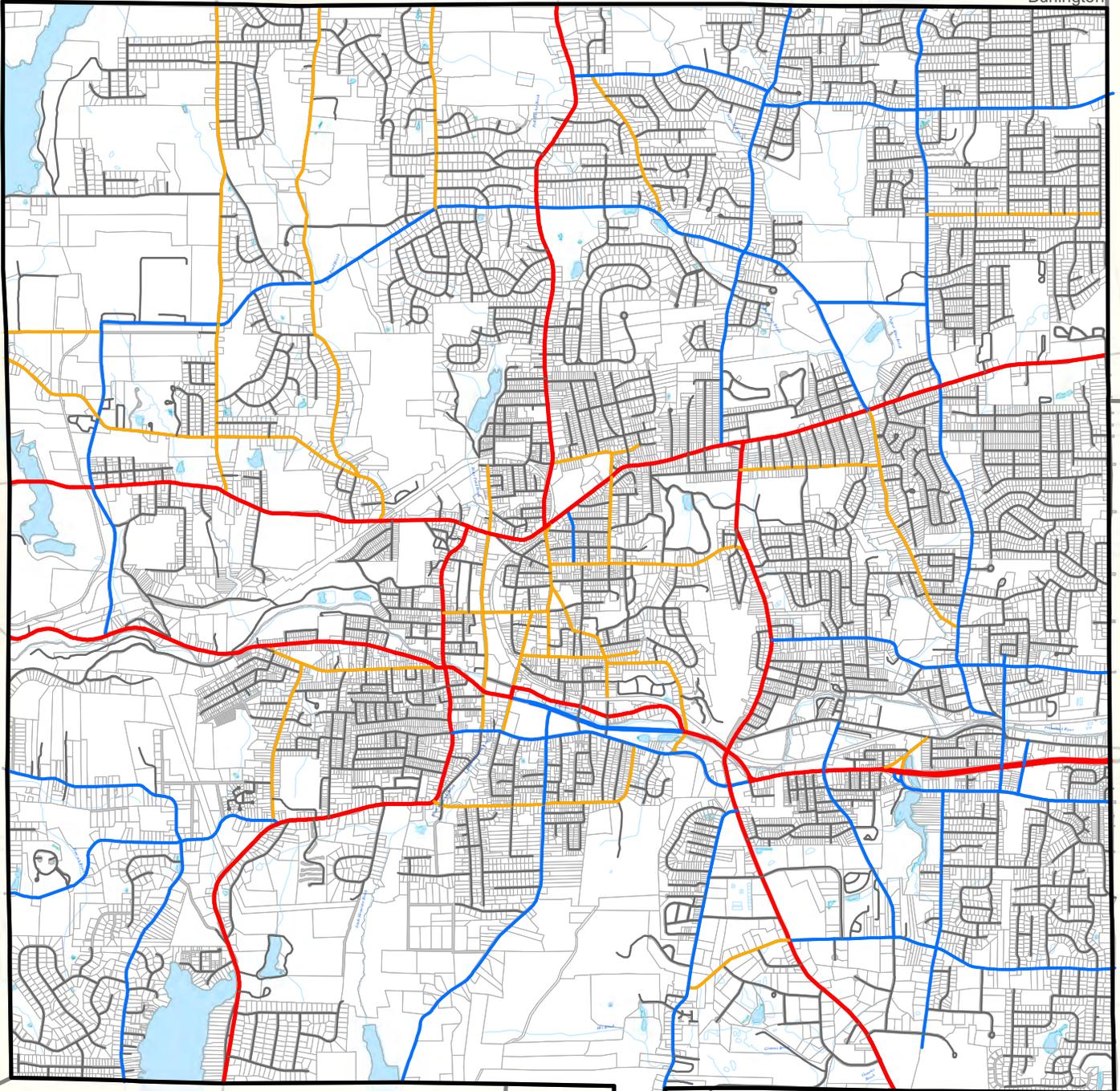
	Arterial / Commercial / Industrial	Collector	Local Residential
R.O.W. Width	60 feet	50 feet	
Width of Pavement	40 feet	34 feet	30 feet if through street 30 feet if temp. dead end 28 feet if dead end > 650 feet 26 feet if dead end < 650 feet
Width of Sidewalk	5 feet		4 feet
Curb	Machine formed bituminous concrete Granite or pre-cast conc. on radii of less than 75 feet		
Maximum Grade	5 %		10 %
Minimum Grade	1 %		
Max. Length of Max. Grade	600 lineal feet		850 lineal feet
Min. Centerline Radius	500 feet		200 feet
Min. Street Line Radius at Intersections	25 feet		15 feet
Turnaround R.O.W.	140 feet dia.	100 feet dia.	
Turnaround Pavement	120 feet dia.	84 feet dia.	
Max. Grade Turnaround	3 %		5 %

Road Classifications Map

Bristol, CT

DRAFT

Burlington



Wolcott

Southington

Legend

-  Major Arterial
-  Minor Arterial
-  Collector
-  Local



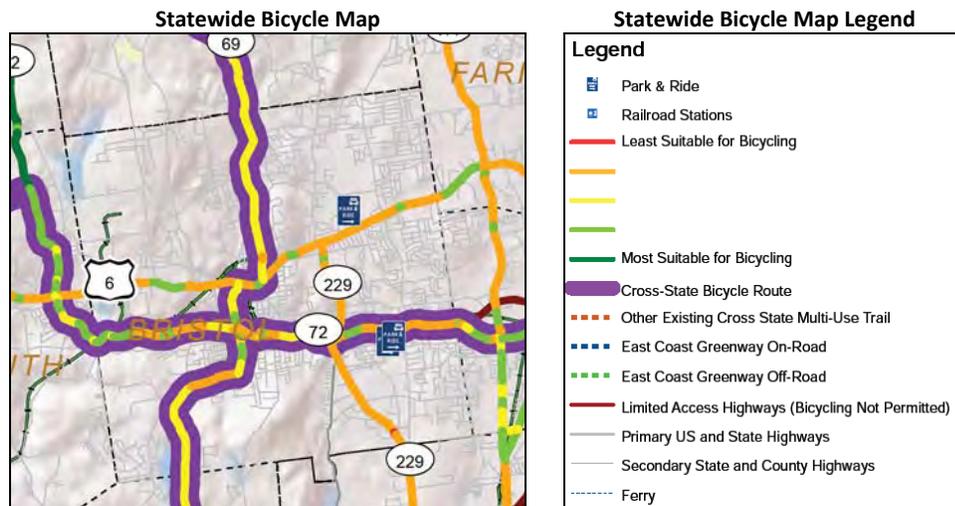
2,700 Feet

Pedestrian Circulation

- Sidewalks exist in many areas of Bristol
- City ordinance requires new sidewalks in commercial and industrial areas unless deferred by the City Council
- The subdivision regulations provide that sidewalks shall be installed on both sides of all proposed streets in subdivisions for the entire length of the street unless deferred by the Commission
- The zoning regulations provide that pedestrian walkways shall be provided in such locations as to separate pedestrian movement from vehicular movement wherever feasible
- Maintenance and repair of sidewalks is the responsibility of the abutting property owner
- There is presently no comprehensive inventory of off-road pedestrian routes (trails) in Bristol.

Bicycle Circulation

- The Connecticut Department of Transportation's *Statewide Bicycle and Pedestrian Transportation Plan* (2009) suggests potential bicycle suitability on state highways based on shoulder width and traffic volume
- There is presently no comprehensive inventory of potential bicycle routes on City streets or off-road bicycle routes in Bristol.

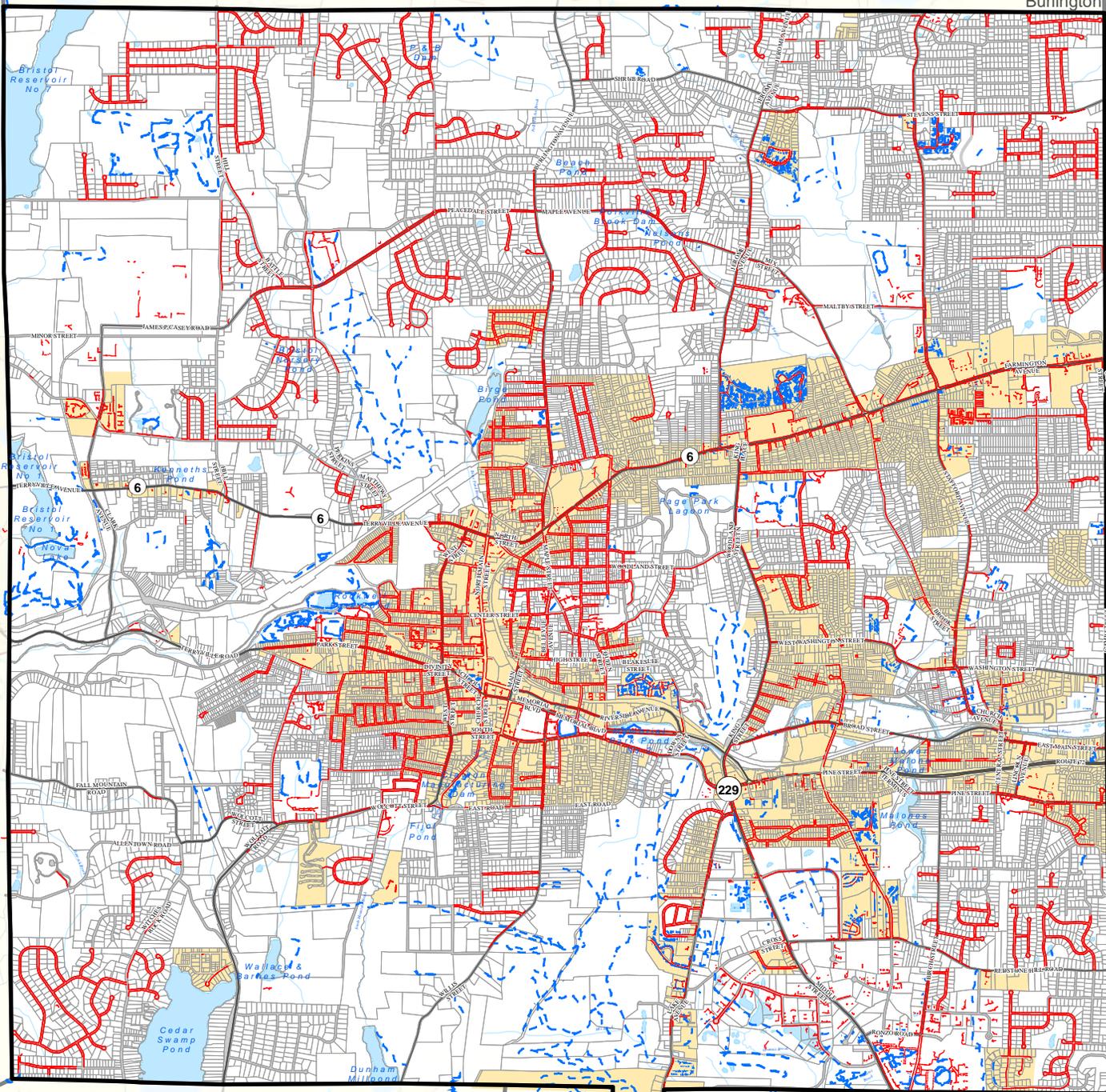


Pedestrian Areas Map

Bristol, CT

DRAFT

Burlington



Wolcott

Southington

Legend

-  Sidewalks
-  Trails
-  High Priority Pedestrian Areas



2,700 Feet

Bus Transit

- Public bus transit in Bristol consists of both a fixed route system and a commuter bus system.
- The fixed route system is operated by Connecticut Transit.
 - The “Bristol Local” service runs during the day between downtown and Tunxis Community College in Farmington via Route 6 with stops at the Senior Center and Bristol Hospital and some housing complexes (transfers are available at Tunxis Community College to other destinations)
 - The “Plainville-Bristol” service runs during the day from New Britain to Bristol on Route 72.
 - There are some route variations during the day and on weekends.
- The commuter bus system (Commuter Express Service) is provided for Connecticut Transit by private bus companies with service to and from downtown Hartford and with stops at commuter lots on Route 229 at Lake Avenue and on Todd Street.
- Dial-a-ride service is provided by the Central Connecticut Paratransit Service for Bristol residents with a physical or other disability and includes all or part of nine cities and towns.
- Bristol’s bus transportation system is underutilized.
- Most workers living in Bristol drive, rather than take the bus, to their place of employment.

Rail Transit

- Bristol has a rail line running through it but passenger rail service and trolley service (streetcar) have not been available here for decades
- The Connecticut Department of Transportation is conducting a study of railroad potential in the Hartford-Waterbury rail corridor
- While the likely first step will be to upgrade the rail line to address freight needs, there is potential that the line will also be considered for the establishment of passenger service for all or a portion of the corridor

GOAL

Provide for a comprehensive and integrated transportation system to best meet the needs of Bristol residents, businesses, and visitors.

Policies:

1. Promote an adequate, safe, and balanced transportation system which includes:
 - a. roads,
 - b. sidewalks and trails,
 - c. bicycle routes, and
 - d. public transit.
2. Promote a “complete streets” approach to ensure that local roadways are designed to accommodate multiple modes of transportation (motor vehicles, pedestrians, bicycles, public transit, etc.).
3. Encourage the City, region and State to address transportation issues in a coordinated manner.

GOAL

Establish and maintain a roadway network which provides for the safe, efficient and orderly movement of people and goods and **provides for** adequate access to places of employment, residential, recreational and commercial activity.

Policies:

4. Provide for adequate traffic circulation within and between all areas in Bristol.
5. As part of new subdivision development, ensure that strategically located through streets continue to be established in order to provide for traffic flow through the overall neighborhood and the community.
6. Seek to improve traffic conditions on Bristol's major arterial streets (Routes 6, 69, 229 and 72).
7. Utilize access management and similar techniques, as appropriate, to reduce traffic congestion, with particularly along Routes 6, 229 and 72.
8. Monitor intersections and road segments with elevated accident experience and/or congestion and seek to improve streets and intersections to address identified issues.
9. Utilize pavement management programs to maintain roadway pavement as efficiently and cost-effectively as possible.
10. Continue to coordinate traffic signalization and traffic markings.
11. Seek opportunities to reduce street pavement widths where feasible and appropriate.
12. Recommendations:
13. Extend the applicability of the access management provisions in the "Access Management Overlay Zone" to the business sections of the Route 6 and Route 229 corridors.
14. Encourage the Mayor and City Council to establish the position of a professional traffic engineer.

GOAL

Promote pedestrian circulation.

Policies:

15. Require the provision of sidewalks in appropriate areas.

Recommendations:

16. Prepare a comprehensive sidewalk plan.
17. Develop and maintain a comprehensive inventory of sidewalks and walking trails as a resource for Bristol residents.

GOAL

Seek to enhance the opportunities for bicycle circulation and mobility.

Policies:

18. Establish a network of bicycle routes within Bristol.
19. Become recognized as a bicycle-friendly community.

Recommendations:

20. Stripe existing and future bicycle routes to clearly designate bicycle lanes.
21. Provide bicycle storage facilities in commuter parking lots.
22. Develop and maintain a comprehensive inventory (and rating) of bicycle routes as a resource for Bristol residents.
23. Seek recognition as a "bicycle-friendly community" by the League of American Bicyclists.

GOAL

Improve public transit options available to Bristol residents and businesses.

Policies:

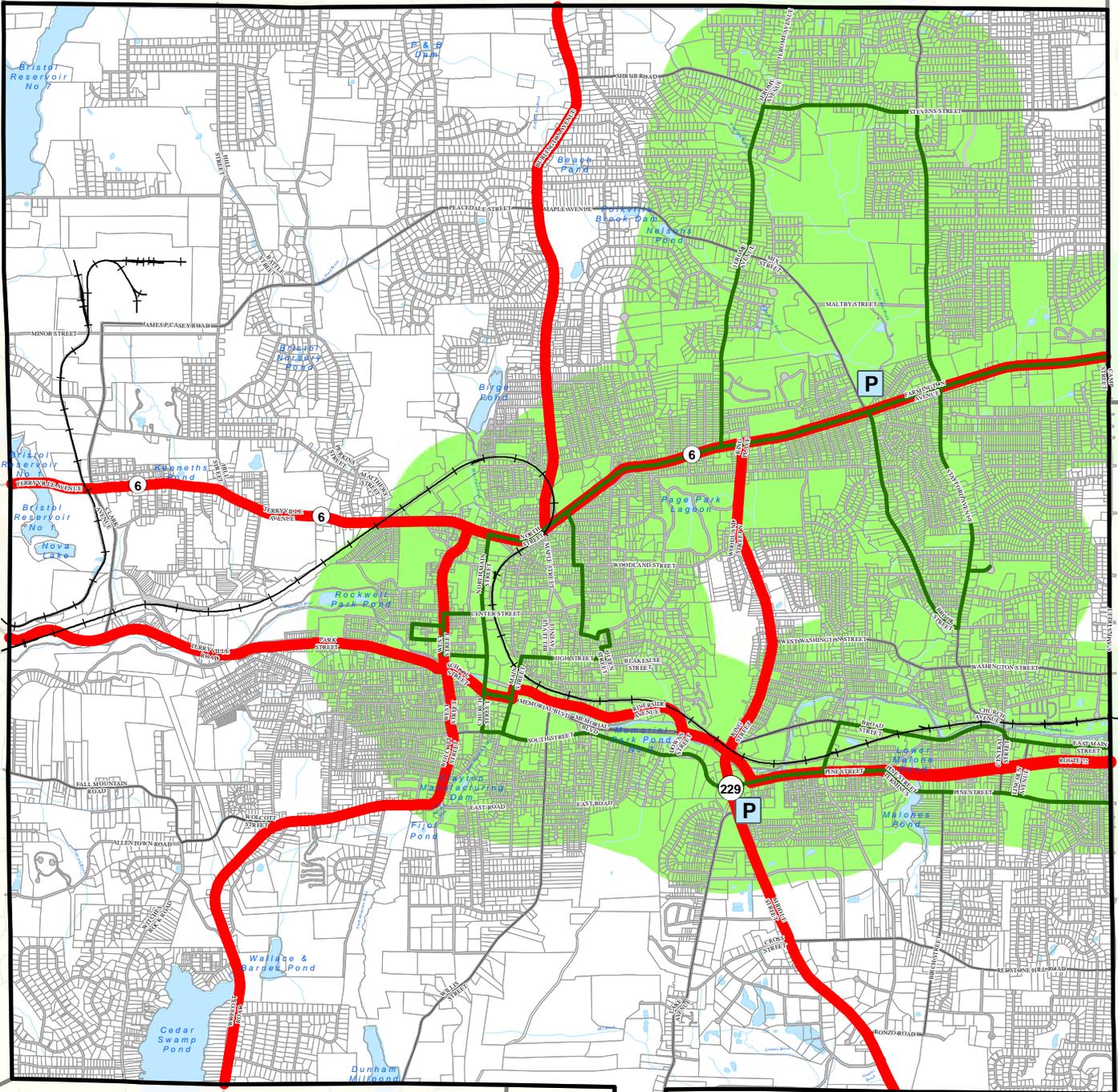
24. Encourage more frequent bus service on existing routes.
25. Continue to encourage bus routes that better connect high concentrations of people with employment centers, shopping areas and entertainment destinations.
26. Continue to support commuter bus service to downtown Hartford.
27. Promote establishment of passenger rail service in Bristol.
28. Encourage the establishment of a multi-modal transportation center in downtown Bristol.
29. Continue to support the availability of dial-a-ride service for those who need it.

Other Transportation

Bristol, CT

DRAFT

Burlington



Wolcott

Southington

Legend

- +— Rail Lines
- State Identified Bike Routes
- Bus Routes
- Transit Service Area
- P Park and Ride Lot



Possible Utility Strategies

Overview (adapted from 2000 POCD)

The nature, extent and condition of a community's infrastructure all contribute significantly to its quality of life.

This chapter summarizes the major components of the public infrastructure in Bristol that directly affect – and/or are affected by – land use and development policies and decisions; these include ... water service and sewer service.

Key Findings, Conclusions and Issues

Sewage Disposal

- Municipal sewers serve most areas of Bristol.
- City land use regulations encourage or require the use of sewers in most areas
- Future sewer extensions will generally be provided by private developers
- Those areas without sewer service utilize on-site septic systems.

- The Water Pollution Control Facility (WPCF) has an average flow of 8 MGD, design capacity of 10.75 MGD, and peak capacity of 33 MGD.
- Plant upgrades to meet new phosphorus standards are being made

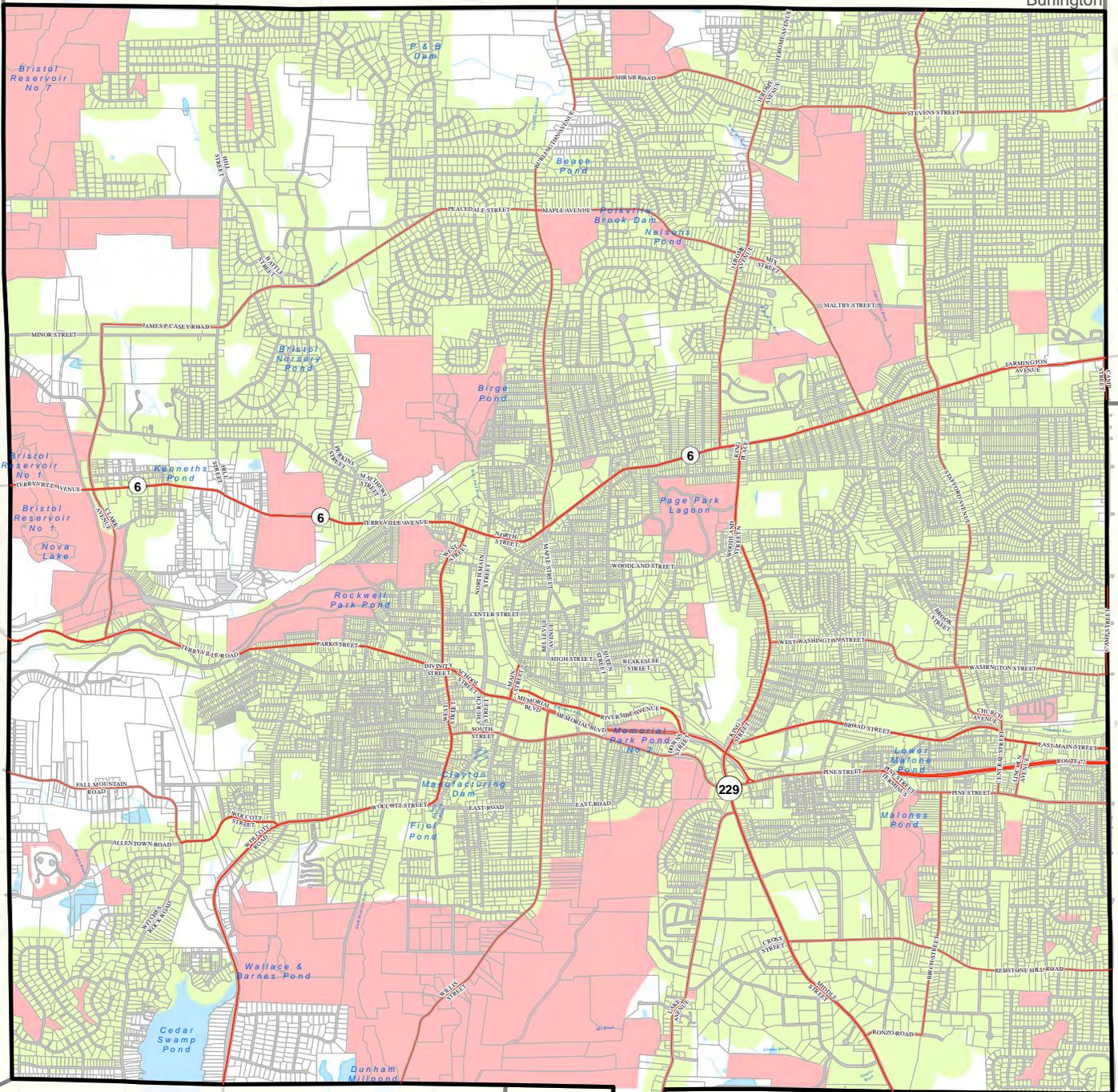
- The main issue is inflow and infiltration of non-sewer water into the sewer lines (from groundwater, basement drains, and other sources)
- This issue is not unique to Bristol but is exacerbated by the age of some of the sewer pipes and the methods used when they were constructed
- The City has purchased a “grout truck” which is used to inspect the sewer pipes and repair leaks and other issues

- The last major upgrade of the WPCF was in 1987 (almost 30 years ago) and additional maintenance and upgrades to the facility will likely be needed in the future.
- As a result of improvements in water conservation, the capacity of the WPCF is expected to be adequate for community needs during the planning period

Sewer Service Area Map

Bristol, CT

Burlington



Wolcott

Southington

Legend

-  Sewer Avoidance Area
-  Existing Sewer Service Area
-  Potential Future Sewer Service Area



2,700 Feet

Water Supply

- The Bristol Water Department operates the public water supply system.
- City land use regulations encourage or require the use of public water in most areas
- Future water extensions will generally be provided by private developers

- The total capacity of the system is 14 million gallons per day (MGD)
- Public water exists in almost all areas of Bristol and serves approximately 85% of the population.

- Water is obtained from a surface water reservoir system and from five groundwater wells.
- The Water Department owns land around the reservoirs and wells to protect the water supply and continues to explore ways to enhance this protection
- The Water Department operates a series of storage tanks to serve the different elevations within their service area

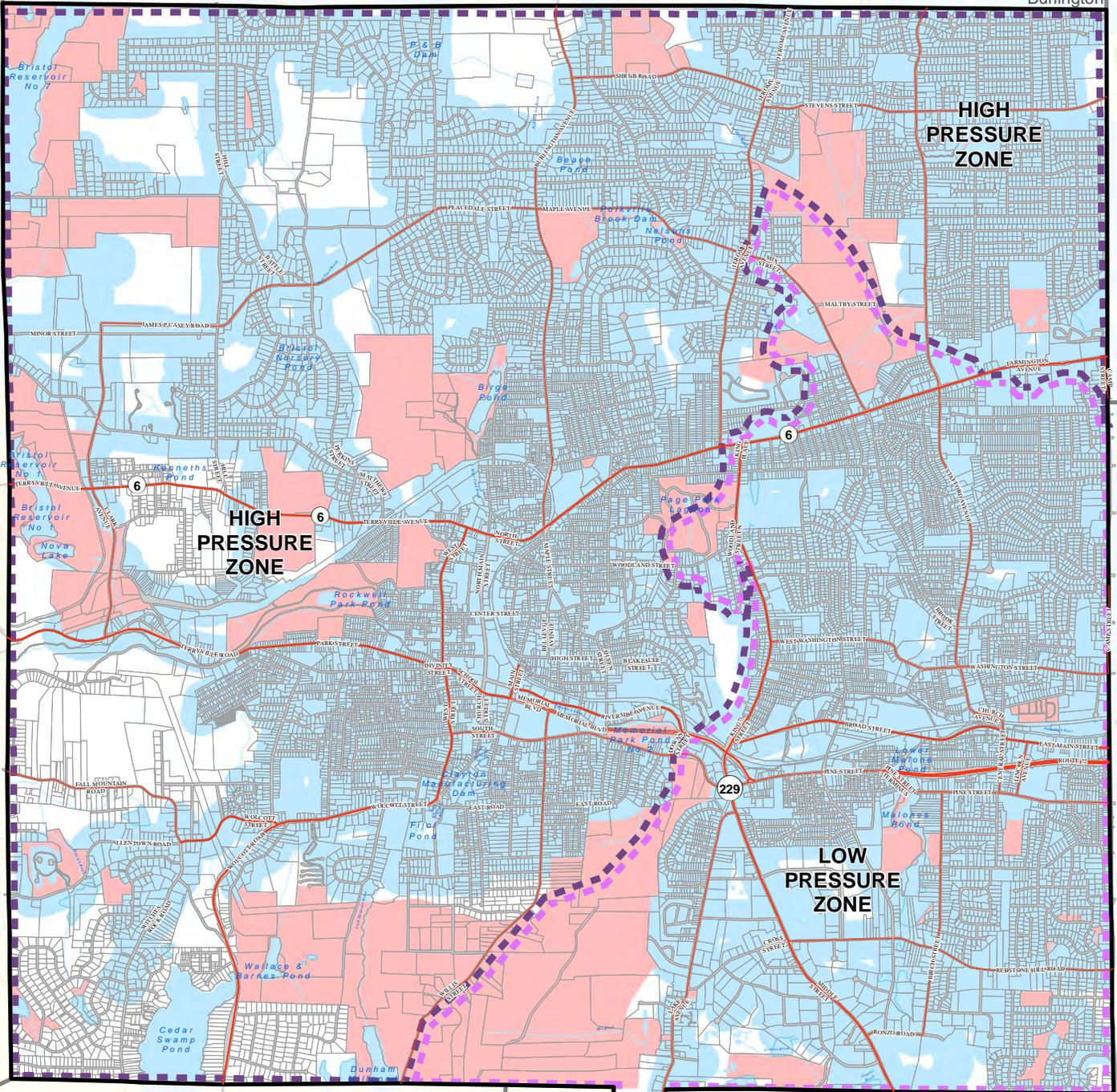
- As a result of improvements within the system and a trend towards water conservation, the capacity of the water supply system is expected to be adequate for community needs during the planning period
- However, if base stream flow regulations are adopted which require minimum releases from reservoirs to maintain stream habitats, the adequacy of the water system may need to be re-evaluated

Water Service Area Map

Bristol, CT

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Burlington



Wolcott

Southington

Legend

-  Existing Water Service Area
-  Water Service Avoidance Area
-  Potential Future Water Service Area



2,700 Feet

Stormwater Drainage

- Stormwater drainage is becoming an issue of increasing interest and concern around the state and the nation
- The Public Works department maintains the drainage system within City streets
- The City has a “stormwater trust” which manages some of the detention basins installed over the years to help manage runoff
- More attention is being paid to “low Impact development” and other strategies which seek to improve water quality and manage water quality better by encouraging infiltration of rainwater and treatment at locations closer to where the raindrop falls
- This is a change from historic drainage approaches which attempted to treat rainfall through a piped conveyance system with very little treatment

Wireless Communications

- A significant number of households have foregone traditional “land lines” and now rely on cellular telephone service for communications
- This situation is expected to continue and may accelerate in the future
- Ensuring that there is adequate wireless coverage to serve the needs of Bristol residents and businesses may be an important consideration in the future

Energy

- In 2008, the City established an Energy Task Force to identify “actions which the task force determines to have the best potential to reduce the cost of energy spending by the City, and reduce the amount of global warming pollution produced by the City”
- Then, in 2011, the City of Bristol commissioned a “comprehensive energy audit” to help understand the energy usage of municipal facilities and to explore ways to become more energy efficient in the future
- In 2013, the City also launched an energy saving program which offered homeowners and renters an opportunity to evaluate their energy utilization

GOAL

Support the provision of water, sewer, and other utility services to serve the needs of the community.

Policies:

1. Continue to maintain infrastructure facilities such as sanitary sewers and storm drainage to help meet community needs.
2. Continue efforts to maintain the capacity of the Water Pollution Control Facility and address inflow and infiltration issues.
3. Continue to protect the integrity of the water supply system.
4. Ensure an adequate supply of water to accommodate the needs of both the existing community and future development.
5. Discourage the extension of public water lines and sanitary sewers into the south-central section of the City dominated by South Mountain, in order to reduce the pressure there for higher-density residential development and to preserve this environmentally sensitive, significant topographical feature.
6. Encourage the provision of wireless communications coverage to serve the needs of Bristol residents and businesses.
7. Seek to participate with
8. Continue to promote energy efficiency in municipal programs and facilities.
9. Investigate the desirability of establishing a micro-grid within downtown Bristol and/or other means of generating energy to meet baseline or emergency needs in the community.
10. Program capital improvements on the basis of a priority system related to the needs of the community and integrated with the Plan.
11. Carefully consider the City's financial resources in the planning for future municipal improvements and resources.

Recommendations:

12. Adopt regulations to require consideration of a "low impact development" approach to stormwater management as part of new development, when appropriate.
13. Promote the underground installation of all utility services to reduce the amount of "visual clutter" created by overhead wires.

