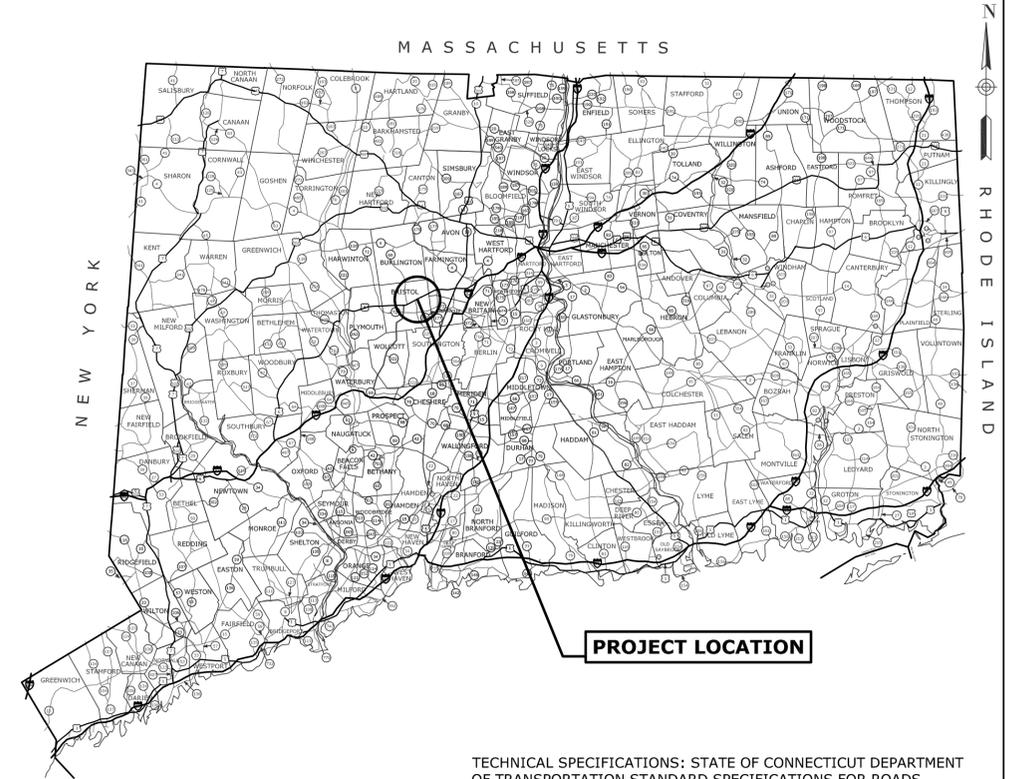


LOCATION MAP
SCALE: 1" = 500'

CITY OF BRISTOL CONNECTICUT

PLANS FOR
BRACE AVENUE DRAINAGE IMPROVEMENTS
VINE ROAD TO CAMP STREET
TO BE MAINTAINED BY THE CITY OF BRISTOL
PROJECT NO. 2C25-068



TECHNICAL SPECIFICATIONS: STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES, AND INCIDENTAL CONSTRUCTION (FORM 819 DATED 2024) AND ALL LATEST SUPPLEMENTAL SPECIFICATIONS OR LATEST AT THE TIME OF BID THERETO, AS WELL AS ANY SPECIAL PROVISIONS BY THE CITY OF BRISTOL.

SURVEY: ALL COORDINATES ON THE PROJECT ARE BASED ON 1983 N.A.D. ALL ELEVATIONS ARE BASED ON 1988 N.A.V.D.

CONNECTICUT DEPARTMENT OF TRANSPORTATION OR CITY OF BRISTOL BIDDING AND OTHER INFORMATION AND DOCUMENTS WHICH ARE OBTAINED THROUGH THE INTERNET, WORLD WIDE WEB SITES OR OTHER SOURCES ARE NOT TO BE CONSTRUED TO BE OFFICIAL INFORMATION FOR THE PURPOSES OF BIDDING OR CONDUCTING OTHER BUSINESS WITH THE CITY OF BRISTOL.

IT IS THE RESPONSIBILITY OF EACH BIDDER AND ALL OTHER INTERESTED PARTIES TO OBTAIN ALL BIDDING RELATED INFORMATION AND DOCUMENTS FROM OFFICIAL SOURCES WITHIN THE CITY OF BRISTOL.

PERSONS AND/OR ENTITIES WHICH REPRODUCE AND/OR MAKE SUCH INFORMATION AVAILABLE BY ANY MEANS ARE NOT AUTHORIZED BY THE TOWN OF GREENWICH TO DO SO AND MAY BE LIABLE FOR CLAIMS RESULTING FROM THE DISSEMINATION OF UNOFFICIAL, INCOMPLETE AND/OR INACCURATE INFORMATION.

LEGEND:			
	Edge of Pavement		Iron Pin (Found)
	B.C.L.C.		Monument (Found)
	Stone Wall		Sign
	Ledge Outcrop		Manhole
	Inland Wetland Limits		"C" Catch Basin
	Tree Line		"C-L" Catch Basin
	Shrub		Utility Pole
	Evergreen Tree		Light Pole
	Deciduous Tree		Metal Post
	Street Line		Guy Anchor
	Property Line		Water Gate
	Chain Link Fence		Gas Valve
	Pipe Fence		Gas Meter
	Piping (San., Stm.)		Mail Box
	Gas		
	U/G Elec. Line		
	Water Line		
	Overhead Utilities		
	U/G Tele. Line		

LIST OF DRAWINGS	
SHEET NO.	TITLE
1	TITLE SHEET
2 - 4	EXISTING CONDITIONS PLANS
5 - 7	ROADWAY IMPROVEMENT PLANS
8 - 9	SEDIMENTATION CONTROL
10	BRACE AVENUE ROADWAY PROFILE
11	O'SULLIVAN ROADWAY PROFILE
12	KOZANI & VERA ROADWAY PROFILES
13	DRAINAGE DETAILS
14	ROADWAY CONSTRUCTION DETAILS
15	SEDIMENTATION CONTROL

STANDARD DRAWINGS	
DWG. NO.	TITLE
TR-1208-01	SIGN PLACEMENT AND RETRO REFLECTIVE STRIP DETAILS
TR-1208-02	METAL SIGN POSTS AND SIGN MOUNTING DETAILS
TR-1220-01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
TR-1220-02	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES



DESIGNED BY WMC CONSULTING ENGINEERS

SUBMITTED BY STEPHEN R. McDONNELL, P.E. DATE 02/21/25

DIRECTOR OF PUBLIC WORKS - CITY OF BRISTOL

DATE _____
RAYMOND ROGOZINSKI



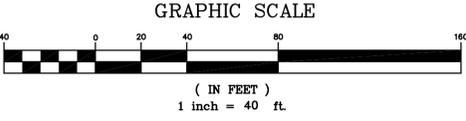
SURVEY NOTES:
 1). ELEVATIONS BASED ON ASSUMED NAVD88.

CERTIFICATION:
 1). THIS MAP AND SURVEY WERE PREPARED IN ACCORDANCE WITH THE REGULATIONS OF CONNECTICUT STATE AGENCIES, SECTIONS 20-300B-1 THRU 20-300B-20, AND THE "RECOMMENDED STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT EFFECTIVE OCTOBER 26, 2018 PREPARED AND ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. PURSUANT TO AND AS SET FORTH THESE STANDARDS; THE TYPE OF SURVEY PERFORMED AND THE MAPPED FEATURES DEPICTED HEREON ARE IN ACCORDANCE WITH THE REQUIREMENTS OF AN IMPROVEMENT LOCATION SURVEY PROPERTY LINES, AS THEY ARE DEPICTED HEREON, PRESENT PROFESSIONAL OPINIONS THAT PERTAIN TO A "DEPENDENT RESURVEY". THIS MAP PRESENTS THE RESULTS OF MEASUREMENTS WHICH WERE MADE UPON THE GROUND IN ACCORDANCE WITH THE ACCURACY STANDARDS OF A CLASS A-2 & T-2 SURVEY.
 2). THIS MAP AND SURVEY WERE PREPARED FOR THE CITY OF BRISTOL TO BE USED IN MATTERS THAT RELATE TO EXISTING CONDITIONS. USE OF THIS MAP FOR OTHER PURPOSES OR BY OTHER PARTIES IS NOT AUTHORIZED OR VALID.
 3). NO DECLARATION IS EXPRESSED OR IMPLIED BY THIS MAP OR COPIES THEREOF UNLESS IT BEARS THE IMPRESSION TYPE SEAL AND ORIGINAL LIVE SEAL AND ORIGINAL LIVE SIGNATURE OF THE SURVEYOR WHOSE NAME AND REGISTRATION NUMBER APPEAR BELOW. ANY CHANGES MADE TO THIS PLAN WITHOUT THE KNOWLEDGE OF THE SIGNERS INVALIDATES THESE DECLARATIONS.

LEGEND

STONE WALL		ELECTRIC MANHOLE	
FENCE LINE		SEWER MANHOLE	
APPRX STREET LINE		TELEPHONE MANHOLE	
EXISTING GAS AS NOTED		ELEC. CABINET	
EXISTING WATER MAINS		LIGHT STD.	
EXISTING SEWER MAINS		CATCH BASINS	
EXISTING DRAINAGE PIPES		HYDRANT	
EXISTING UNGND. ELECTRIC		UTILITY POLE	
GUIDE RAIL			
TELEPHONE LINE			
		GAS BOX / WATER BOX	

THIS MAP AND THE UTILITIES DEPICTED HEREON WERE DERIVED FROM THE BEST AVAILABLE RECORD MAPS AND MARK-OUTS AND ARE APPROXIMATE ONLY AND SHOULD BE FIELD VERIFIED. PLEASE CONTACT "CALL BEFORE YOU DIG" AT 1-800-922-4455. ALL UTILITIES MAY NOT BE DEPICTED.



NO.	DATE	DESCRIPTION
REVISIONS		

FINAL DESIGN



SCALE
 1" = 40'

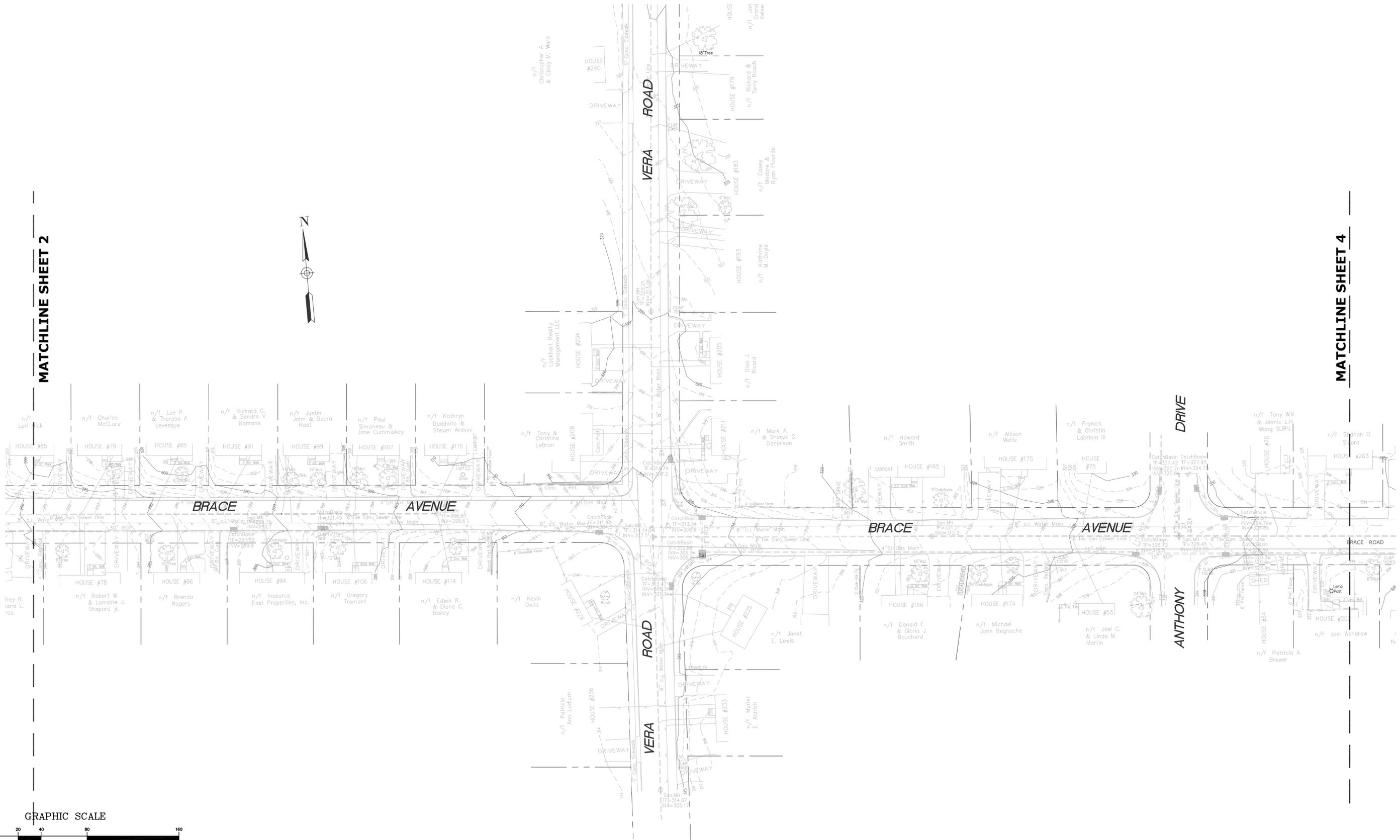
WMC
 CONSULTING ENGINEERS
 WENGELL, McDONNELL & COSTELLO
 87 HOLMES ROAD
 NEWINGTON, CT 06111
 (860) 667-9624

PREPARED FOR
 CITY OF BRISTOL
 150 MAIN STREET
 BRISTOL, CT 06010

EXISTING CONDITIONS PLAN 1 BRACE AVENUE DRAINAGE IMPROVEMENTS BRISTOL, CONNECTICUT			
BRACE AVE DRAINAGE	BRACE_FD	22071.10	2
SIZE	PROJECT	FILE NAME	NUMBER
		REV.	OF

MATCHLINE SHEET 2

MATCHLINE SHEET 4



GRAPHIC SCALE



(IN FEET)
1 inch = 40 ft.

		SUPV.	S.R.M.
		DESIGN	D.T.J.
		DRAWN	D.T.J.
		CHECKED	R.E.B.
		DATE	02/21/25
NO.	DATE	DESCRIPTION	
REVISIONS			

FINAL DESIGN



SCALE
1" = 40'

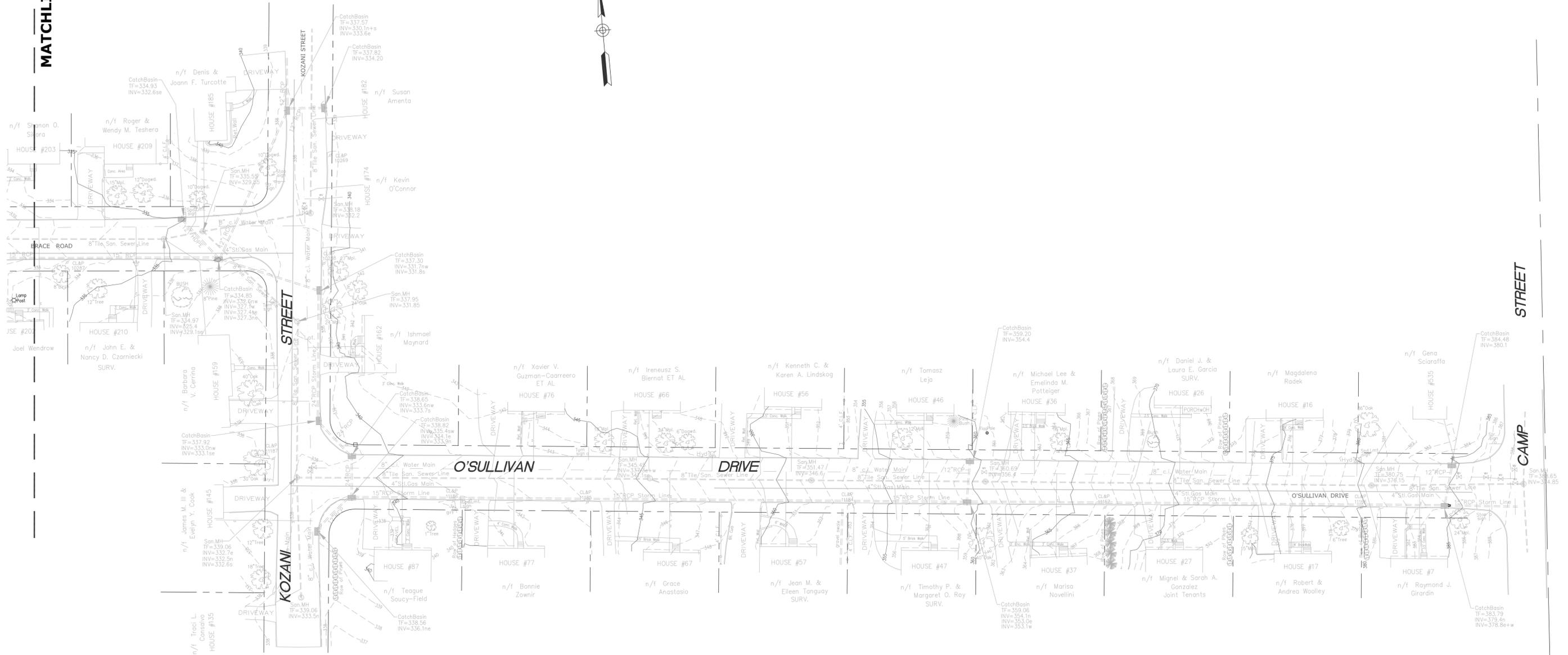


WENDELL, McDONNELL & COSTELLO
87 HOLMES ROAD
NEWINGTON, CT 06111
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PREPARED FOR
CITY OF BRISTOL
150 MAIN STREET
BRISTOL, CT 06010

EXISTING CONDITIONS PLAN 2
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT

D -	BRACE AVE DRAINAGE PROJECT	BRACE_FD -	22071.10 -	REV.	OF	3
SIZE	PROJECT	FILE NAME	NUMBER	REV.	OF	15



GRAPHIC SCALE



(IN FEET)
1 inch = 40 ft.

		SUPV.	S.R.M.
		DESIGN	D.T.J.
		DRAWN	D.T.J.
		CHECKED	R.E.B.
NO.	DATE	DESCRIPTION	DATE
		REVISIONS	02/21/25

FINAL DESIGN



SCALE
1" = 40'



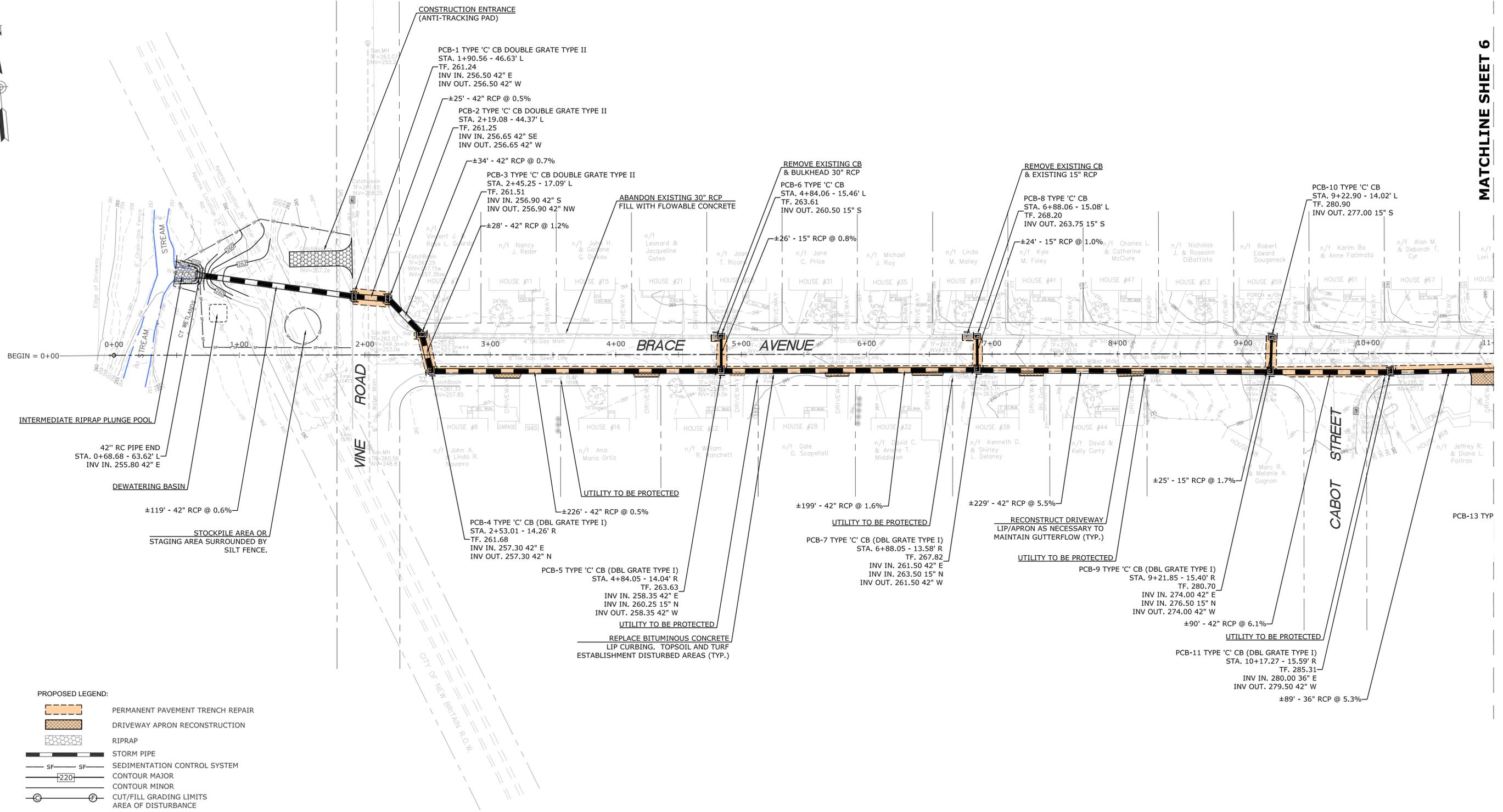
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87 HOLMES ROAD
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(860) 667-9624

PREPARED FOR

CITY OF BRISTOL
150 MAIN STREET
BRISTOL, CT 06010

EXISTING CONDITIONS PLAN 3
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT

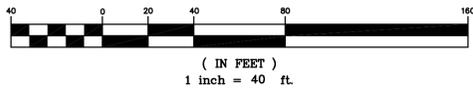
D -	BRACE AVE DRAINAGE	BRACE_FD -	22071.10 -	SHEET	4
SIZE	PROJECT	FILE NAME	NUMBER	REV.	OF
					15



PROPOSED LEGEND:

- PERMANENT PAVEMENT TRENCH REPAIR
- DRIVEWAY APRON RECONSTRUCTION
- RIPRAP
- STORM PIPE
- SEDIMENTATION CONTROL SYSTEM
- CONTOUR MAJOR
- CONTOUR MINOR
- CUT/FILL GRADING LIMITS
- AREA OF DISTURBANCE

GRAPHIC SCALE



		SUPV.	S.R.M.
		DESIGN	D.T.J.
		DRAWN	D.T.J.
		CHECKED	R.E.B.
NO.	DATE	DESCRIPTION	DATE
		REVISIONS	02/21/25

FINAL DESIGN



SCALE
1" = 40'



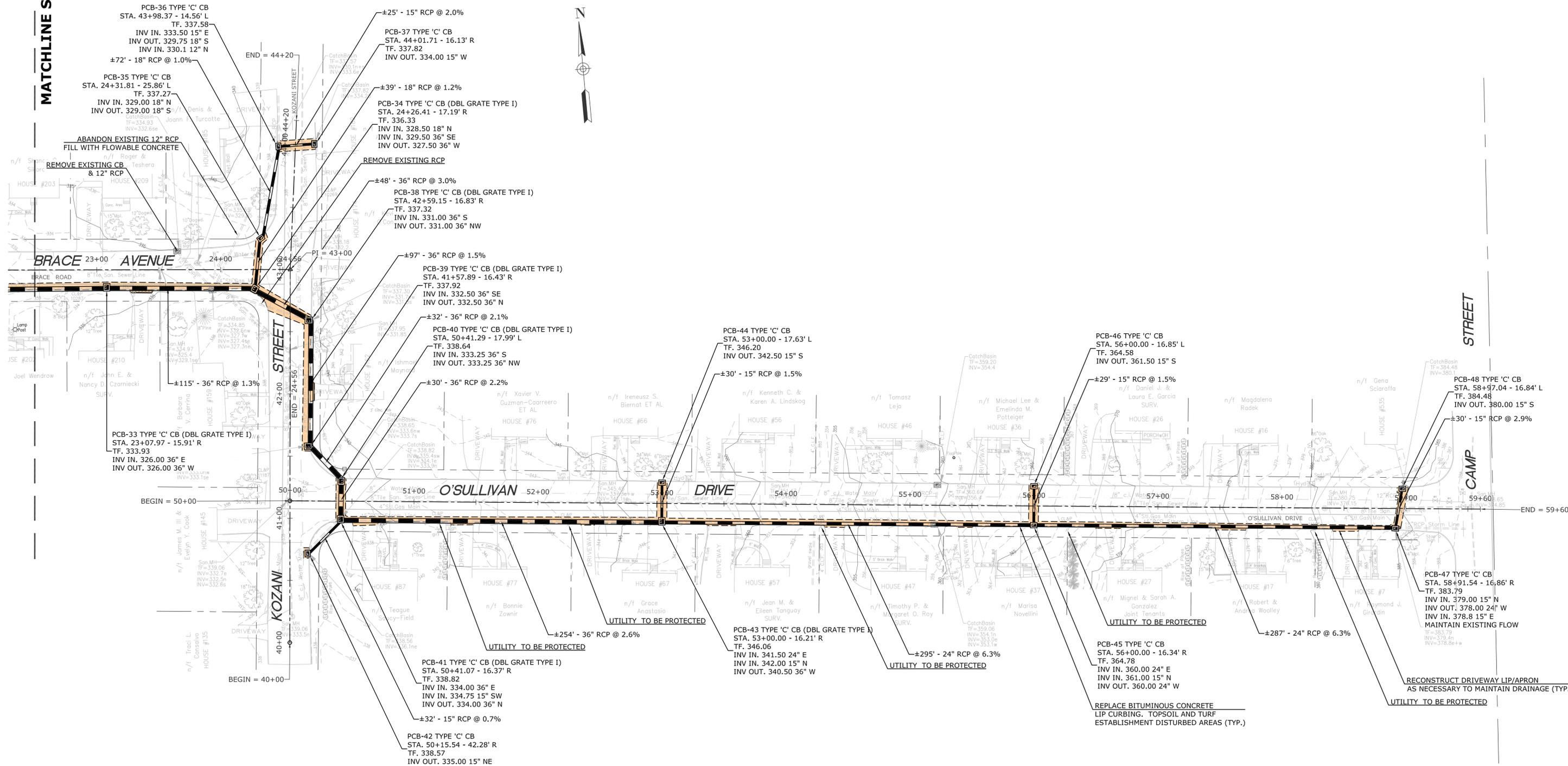
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PREPARED FOR

CITY OF BRISTOL
150 MAIN STREET
BRISTOL, CT 06010

**ROADWAY IMPROVEMENT PLAN 1
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT**

D -	BRACE AVE DRAINAGE	BRACE_FD	22071.10	REV.	SHEET	5
SIZE	PROJECT	FILE NAME	NUMBER	REV.	OF	16



GRAPHIC SCALE



(IN FEET)
1 inch = 40 ft.

NO.	DATE	DESCRIPTION
REVISIONS		

SUPV.	S.R.M.
DESIGN	D.T.J.
DRAWN	D.T.J.
CHECKED	R.E.B.
DATE	02/21/25

FINAL DESIGN



SCALE
1" = 40'



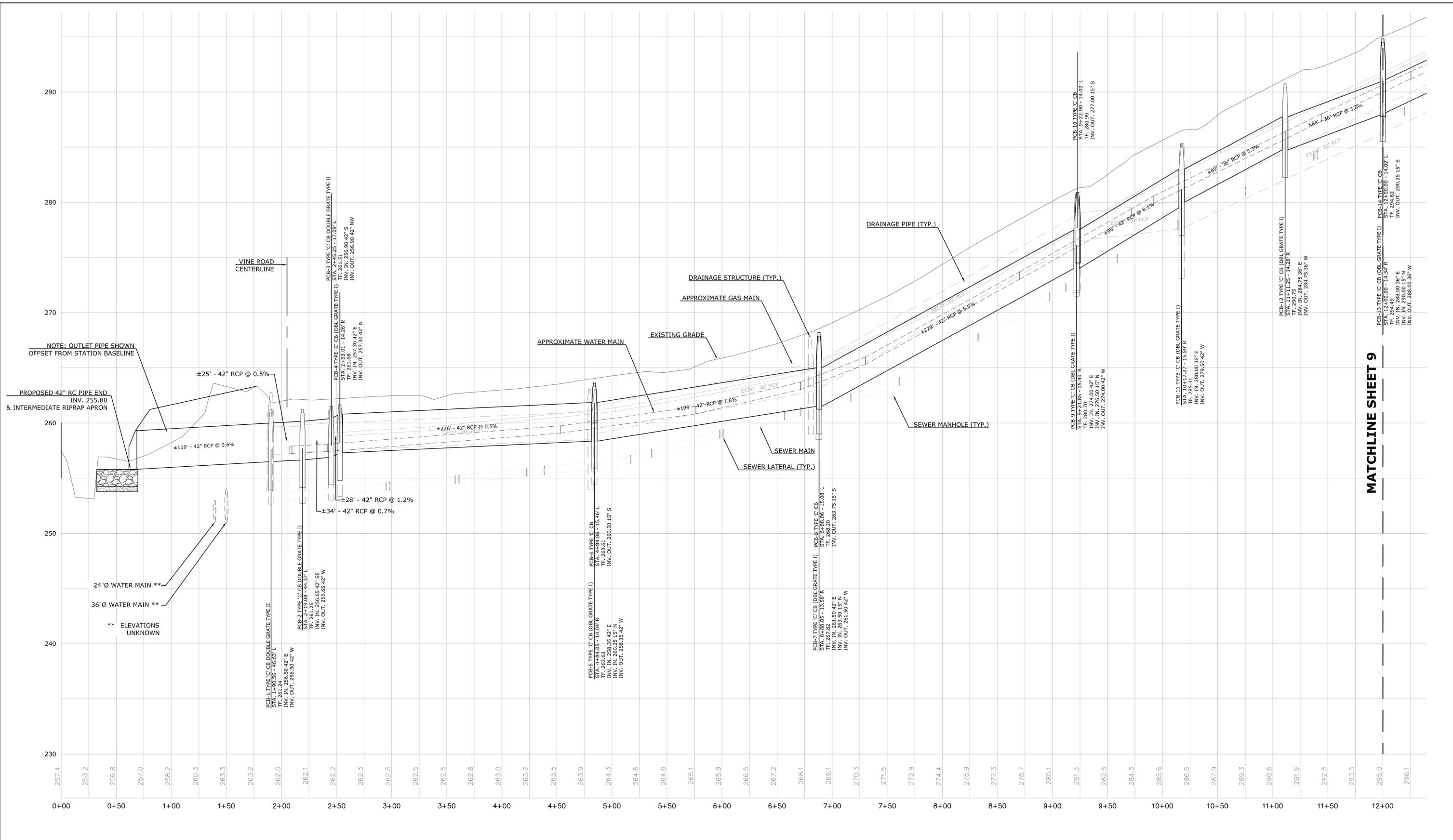
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PREPARED FOR

CITY OF BRISTOL
150 MAIN STREET
BRISTOL, CT 06010

ROADWAY IMPROVEMENT PLAN 3
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT

D -	BRACE AVE DRAINAGE	BRACE_FD	22071.10	REV.	7
SIZE	PROJECT	FILE NAME	NUMBER	REV.	OF
					15



MATCHLINE SHEET 9

NO.	DATE	DESCRIPTION
REVISIONS		

SUPV.	S.R.M.
DESIGN	D.T.J.
DRAWN	D.T.J.
CHECKED	R.E.B.
DATE	02/21/25

FINAL DESIGN



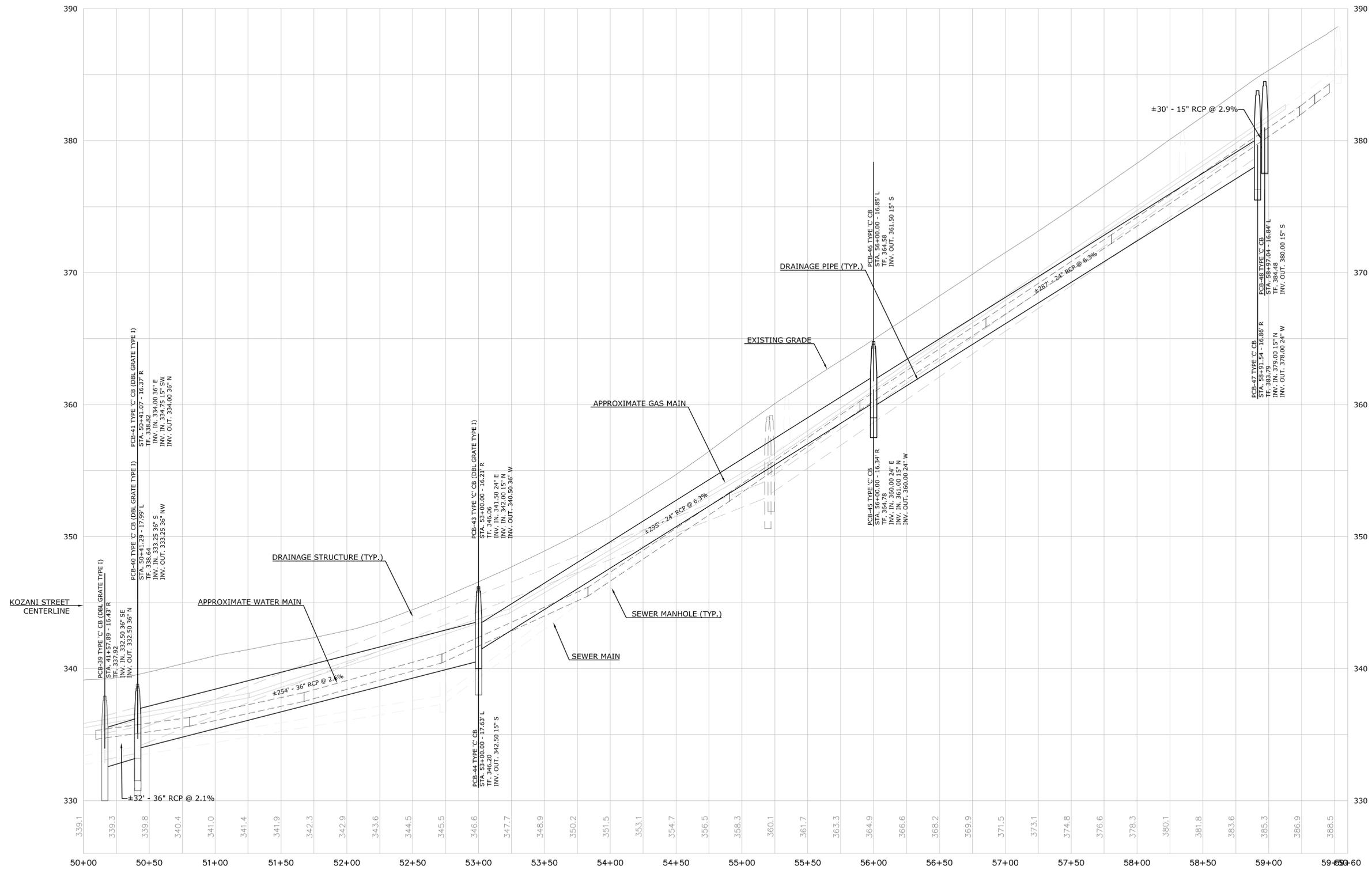
SCALE
HORIZ: 1" = 40'
VERT: 1" = 4'

WMC
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• WENGELL, McDONNELL & COSTELLO •
87 HOLMES ROAD
NEWINGTON, CT 06111
(860) 667-9624

PREPARED FOR
CITY OF BRISTOL
150 MAIN STREET
BRISTOL, CT 06010

BRACE AVENUE ROADWAY PROFILE 1		BRACE AVENUE DRAINAGE IMPROVEMENTS		BRISTOL, CONNECTICUT	
D -	BRACE AVE	-	BRACE_FD	-	22071.10
SIZE	PROJECT	FILE NAME	NUMBER	REV.	OF
					15



NO.	DATE	DESCRIPTION
REVISIONS		

SUPV.	S.R.M.
DESIGN	D.T.J.
DRAWN	D.T.J.
CHECKED	R.B.E.
DATE	02/21/25

FINAL DESIGN



SCALE
 HORIZ: 1" = 40'
 VERT: 1" = 4'

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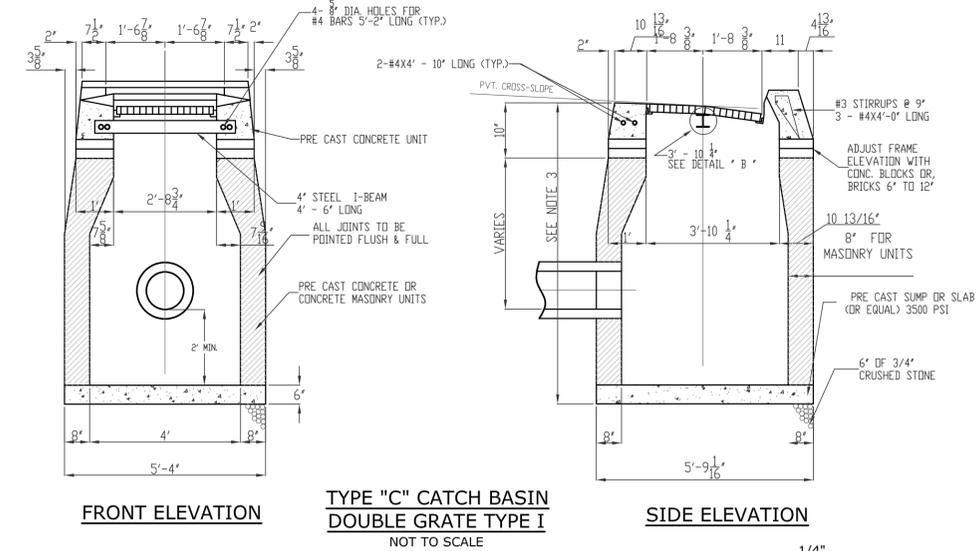
PREPARED FOR
 CITY OF BRISTOL
 150 MAIN STREET
 BRISTOL, CT 06010

**O'SULIVAN DRIVE ROADWAY PROFILE
 BRACE AVENUE DRAINAGE IMPROVEMENTS
 BRISTOL, CONNECTICUT**

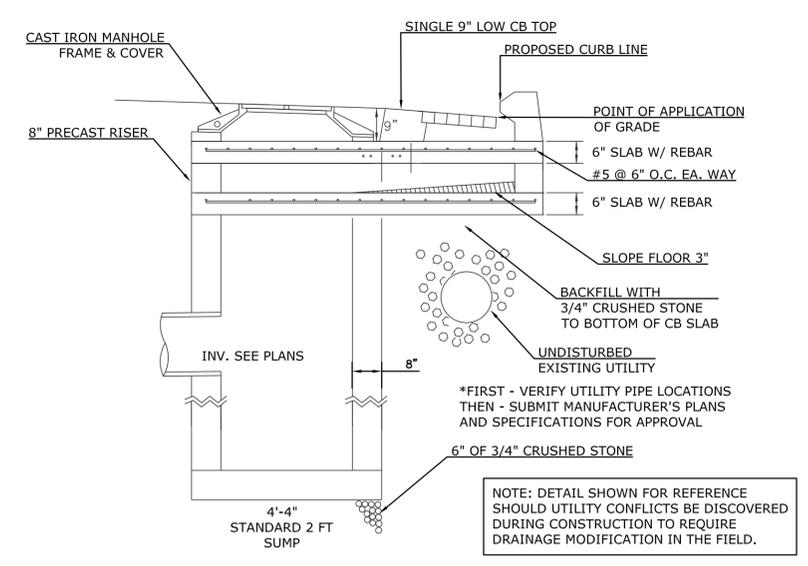
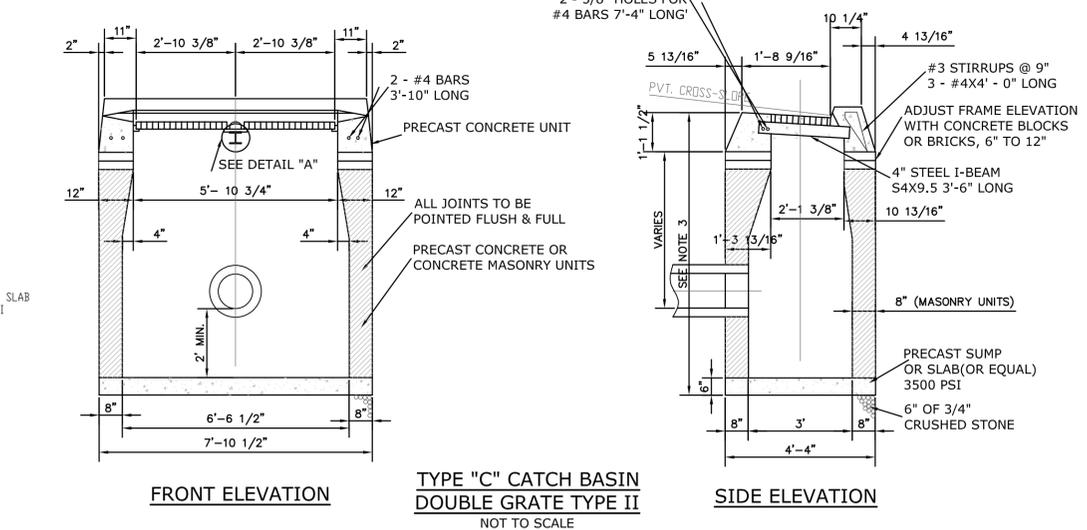
D -	BRACE AVE DRAINAGE	BRACE_FD	22071.10	REV.	OF	15
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SHEET 10

NOTE:
USE C-L GRATE FOR A TYPE C-L DOUBLE GRATE BASIN

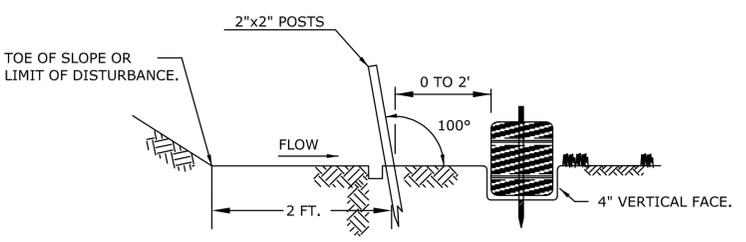
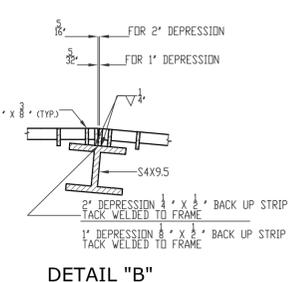
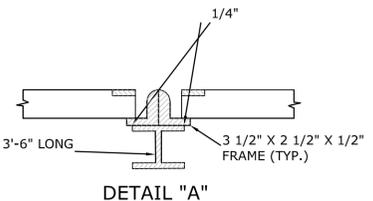


NOTE:
USE C-L GRATE FOR A TYPE C-L DOUBLE GRATE BASIN



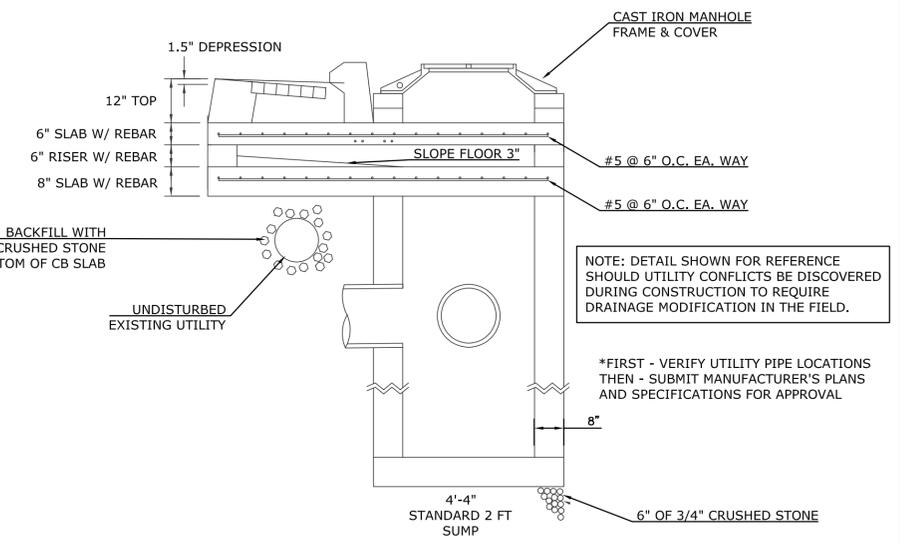
CATCH BASIN NOTES :

1. MINIMUM COVER OVER TOP OF PIPE SHALL BE 2'-0".
2. WALL THICKNESS SHALL BE SUFFICIENT TO MEET HS-20 LOADING.
3. WALL THICKNESS FOR STRUCTURES OVER 10' HIGH IS 12" FOR CONCRETE BLOCK UNITS. INSIDE DIMENSIONS REMAIN THE SAME.
4. ALL PIPES SHALL BE CUT FLUSH WITH INSIDE WALLS.
5. ALL BRICKS SHALL BE CONCRETE.
6. ALL PIPE PENETRATION SHALL BE PARGED SMOOTH TO PROVIDE A WATERTIGHT SEAL BOTH INSIDE AND OUTSIDE THE BASIN
7. INSIDE WALLS OF STRUCTURE TO BE SMOOTH. NO SHELVES ALLOWED.
8. IF 4' SUMP IS NOT POSSIBLE DUE TO UTILITY CONFLICTS, A 2' SUMP MAY BE SUBMITTED FOR THE TOWN ENGINEERS APPROVAL.
9. FRAME AND GRATE SHALL BE GALVANIZED.

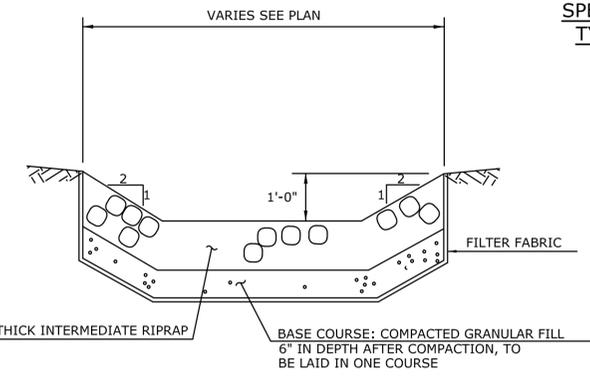


NOTE:
1) TO BE INSTALLED WITHIN UPLAND REVIEW IMPACT LIMITS.

**SPECIAL OFFSET "REAR"
TYPE "C" CATCH BASIN**
NOT TO SCALE

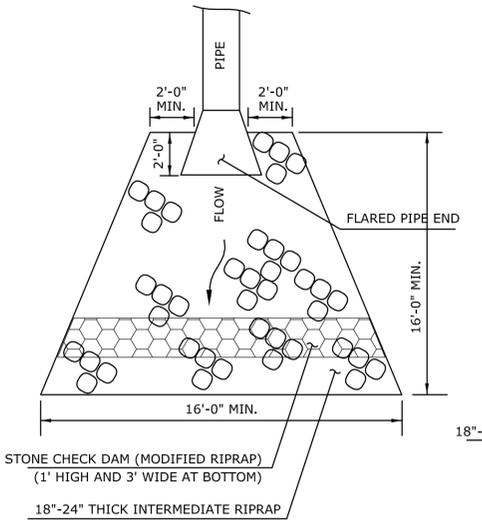
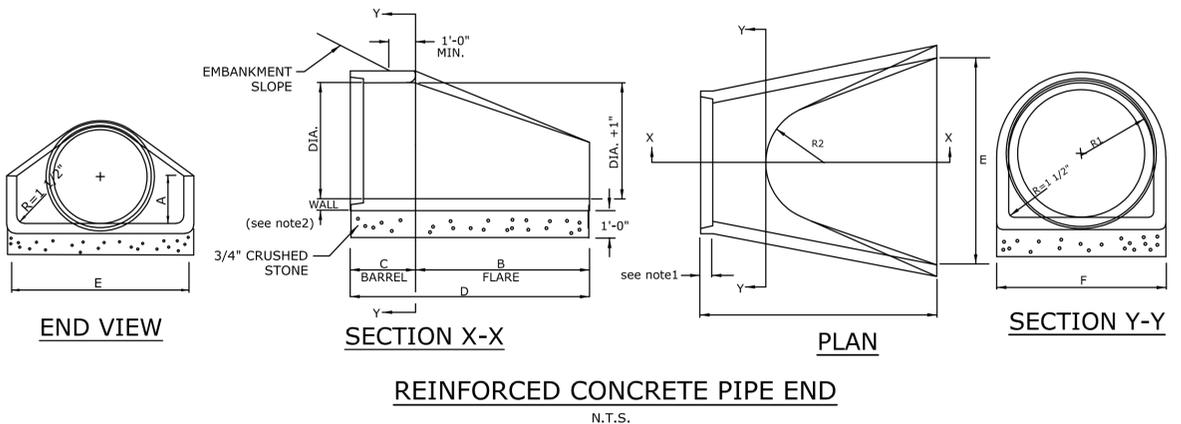


**SPECIAL OFFSET "FRONT"
TYPE "C" CATCH BASIN**
NOT TO SCALE



NOTES:
1) JOINTS SHALL BE TONGUE AND GROOVE OR BELL AND SPIGOT AS REQUIRED TO CONFORM TO PIPE INSTALLED.
2) WALL THICKNESS SHALL CONFORM TO PIPE THICKNESS.

DIMENSIONS FOR REINFORCED CONCRETE CULVERT END								FLARE REINFORCEMENT ONE LAYER ONLY IN CENTER OF WALL		
DIA.	A	B	C	D	E	F	R1	R2	MIN. AREA OF LONGITUDINALS SQ. IN PER FOOT	MIN. AREA OF TRANSVERSE STEEL, SQ. IN. PER FOOT
42"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4'-5 7/8"	2'-3 1/2"	1'-10"	0.108	0.108



RIPRAP SPLASH PAD
N.T.S.

SUPV.	S.R.M.
DESIGN	D.T.J.
DRAWN	D.T.J.
CHECKED	R.E.B.
DATE	02/21/25

FINAL DESIGN



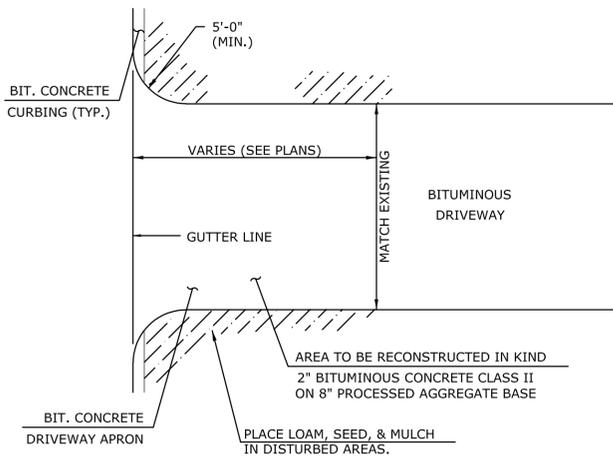
SCALE
NOT TO SCALE

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CONSULTING ENGINEERS
WENGELL, McDONNELL & COSTELLO
87 HOLMES ROAD
NEWINGTON, CT 06111
(860) 667-9624

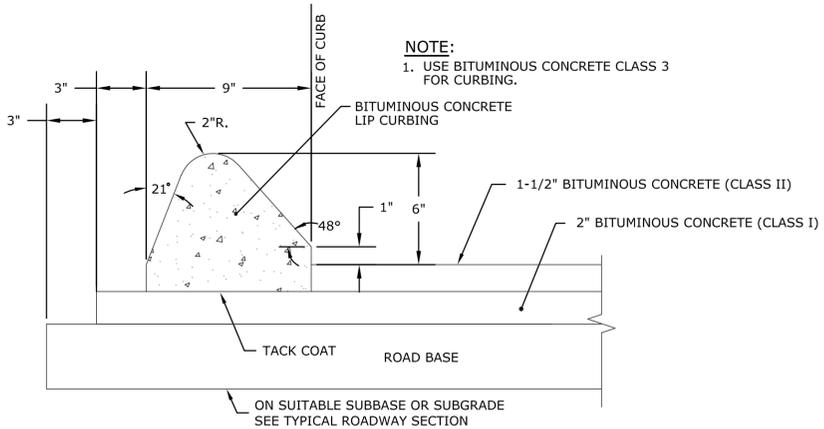
PREPARED FOR
CITY OF BRISTOL
150 MAIN STREET
BRISTOL, CT 06010

**CONSTRUCTION DETAILS
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT**

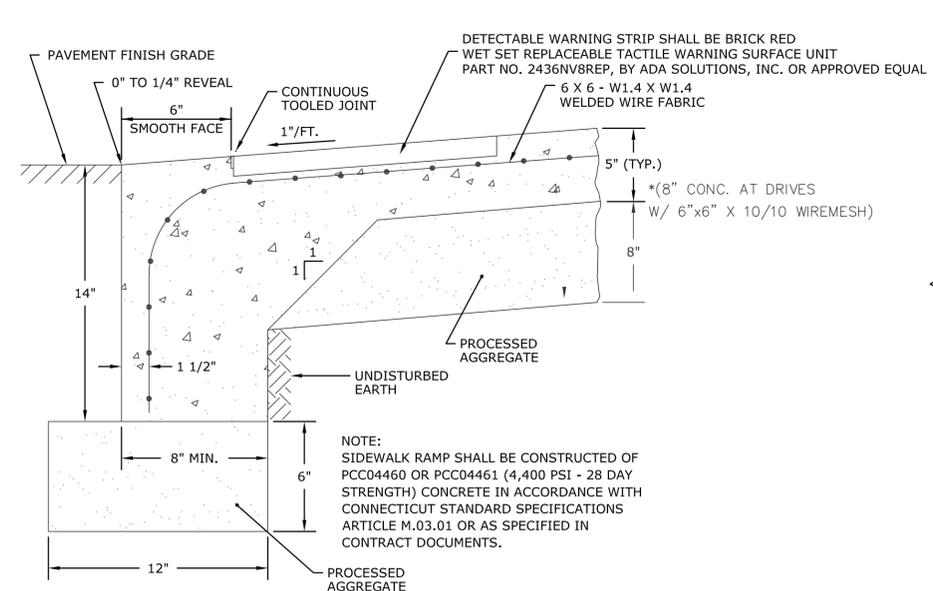
BRACE AVE DRAINAGE PROJECT	BRACE_FD	22071.10	REV.	SHEET	12
SIZE	FILE NAME	NUMBER	REV.	OF	15



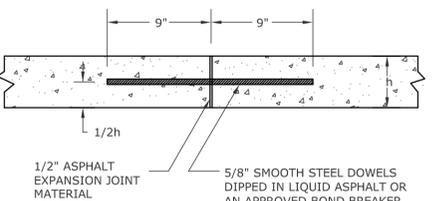
TYPICAL DRIVEWAY PLAN
N.T.S.



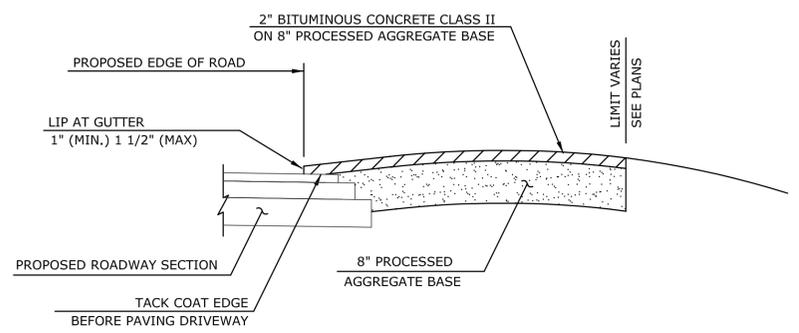
BITUMINOUS CURB SECTION
N.T.S.



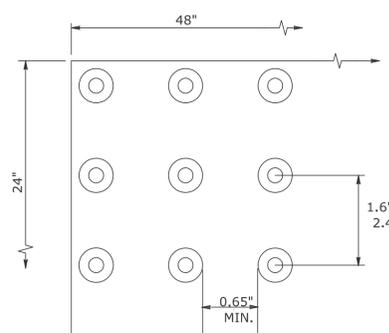
SIDEWALK RAMP SECTION A-A
N.T.S.



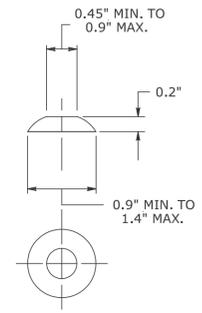
EXPANSION JOINT DETAIL
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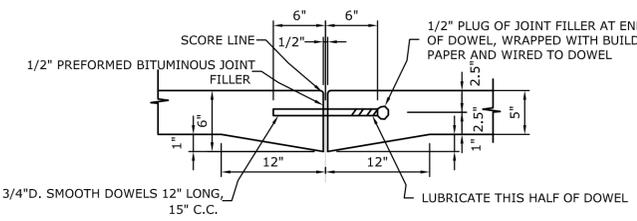
TYPICAL BITUMINOUS DRIVEWAY SECTION
N.T.S.



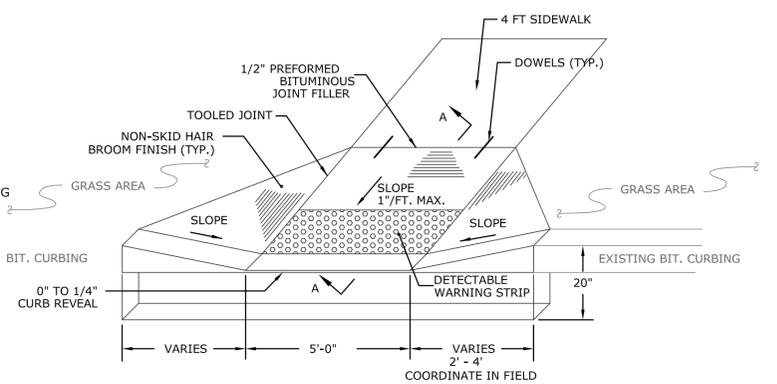
DOME SPACING
N.T.S.



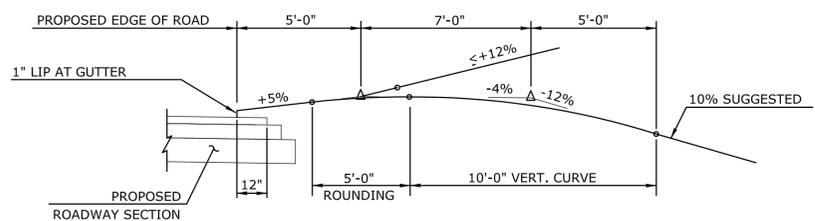
DOME SECTION
N.T.S.



SIDEWALK CONTRACTION JOINT DETAIL
N.T.S.

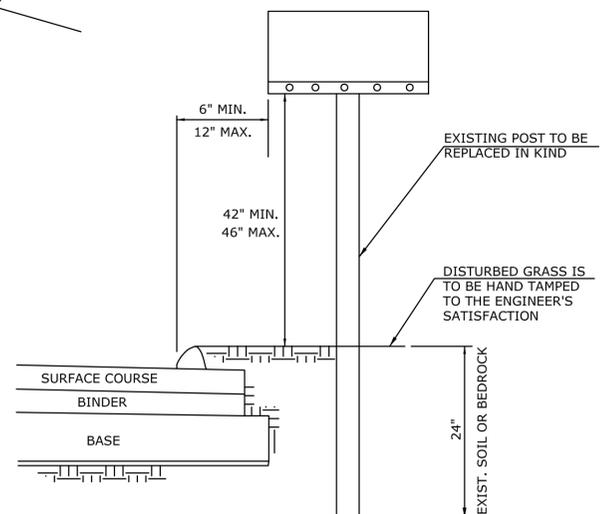


SIDEWALK RAMP (TYPE 13W)
N.T.S.

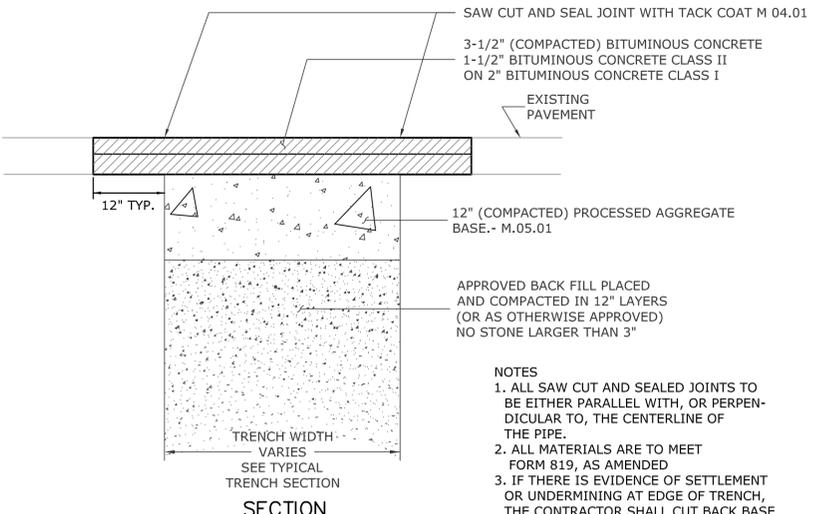


TYPICAL DRIVEWAY PROFILE
N.T.S.

DETECTABLE WARNING STRIP
N.T.S.



MAILBOX AND PAPER TUBE SETTING LOCATION
N.T.S.



PERMANENT PAVEMENT TRENCH REPAIR
N.T.S.

- SIDEWALK RAMP NOTES:**
1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP OR ACCESSIBLE ROUTE SHOULD NOT EXCEED 20:1.
 2. CARE SHALL BE TAKEN TO ASSURE UNIFORM GRADE ON THE RAMP, FREE OF SAGS AND ABRUPT GRADE CHANGES.
 3. ALL RAMP SHALL BE CONSTRUCTED OF PCC04460 OR PCC04461 (4,400 PSI) CONCRETE (28 DAY STRENGTH) CONCRETE IN ACCORDANCE WITH THE SPECIFICATIONS IN THE CONTRACT DOCUMENTS.
 4. SIDEWALK RAMP SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE ALONG ACCESSIBLE ROUTES SHALL BE STABLE, FIRM AND SLIP RESISTANT IN COMPLIANCE WITH ADAAG SECTION 4.5. DETECTABLE WARNING SURFACES SHALL BE INSTALLED FLUSH AT ALL RAMP AND EXTEND 24" MIN. FROM BACK EDGE OF CURBING ALONG TRAVEL WAY.
 5. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION/CONTRACTIONS JOINT OR DUMMY JOINT, 12:1 MAY NOT BE ACHIEVABLE DUE TO SIDEWALK GRADE. IN RECOGNITION OF THIS, A MINIMUM LIMIT OF 15' FOR A PARALLEL RAMP SHALL BE USED. REMOVAL SHALL NOT BE FURTHER THAN 2' FROM THE PROPOSED RAMP UNLESS DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF CONCRETE SIDEWALK.
 6. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 15' UNLESS OTHERWISE NOTED.
 7. SIDEWALK RAMP SHALL BE CONSTRUCTED AND PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", INCLUDING CURBING WITHIN THE LIMITS OF THE NEW SIDEWALK RAMP.
 8. CURBING WITHIN THE LIMITS OF THE NEW SIDEWALK RAMP SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE REQUIREMENTS OF FORM 819 SECTIONS 8.11 AND 8.13.
 9. SIDEWALK RAMP CONFORMING WITH CONNECTICUT GENERAL STATUTES, SEC. 7-118a, SHALL BE INCORPORATED IN ALL PROPOSED SIDEWALKS AT ALL STREET INTERSECTIONS, AND AT ALL OTHER LOCATIONS WHERE THE GRADE OF A DRIVEWAY OR OTHER FACILITY TAKES PRECEDENCE OVER THE GRADE OF THE PROPOSED SIDEWALK.
 10. TRANSITION TO FULL HEIGHT CURB. INSTALL CURB TYPES AS INDICATED ON THE PLANS.
 11. INSTALL THE EDGE OF THE DETECTABLE WARNING 6" FROM EDGE OF ROAD.
 12. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF PEDESTRIAN TRAVEL.
 13. FORMS ARE TO BE SET TRUE TO LINE AND GRADE ON WELL COMPACTED BASE.
 14. PROPER FINISHING PROCEDURE WILL BE FOLLOWED INCLUDING JOINTING, EDGING, AND BROOMING. A FINE BRISTLE BROOM SHOULD BE USED. ALL EDGING TOOL IMPRINTS SHOULD BE STEEL TROWELED PRIOR TO BROOMING.
 15. CURING COMPOUND MAY BE REQUIRED.
 16. PRECAUTIONS ARE TO BE TAKEN TO PROTECT SURFACE FROM DAMAGE.
 17. WALKS SHALL BE BACKFILLED AS SOON AS FORMS ARE REMOVED.
 18. ALL CONCRETE SIDEWALK SLABS SHALL BE RECTANGULAR IN SHAPE. NO FIGURE L SLABS ARE TO BE CONSTRUCTED.
 19. SIDEWALK SLABS SHOULD NOT EXCEED 5' IN WIDTH. IF SIDEWALK SLABS GREATER THAN 5' IN WIDTH ARE TO BE CONSTRUCTED, A LONGITUDINAL EXPANSION JOINT SHALL BE CONSTRUCTED TO FORM ACCEPTABLE SLABS.
 20. INSERT METAL DOWELS AT ALL EXPANSION JOINTS AS SHOWN ON TYPICAL SIDEWALK DETAIL.

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REVISIONS		

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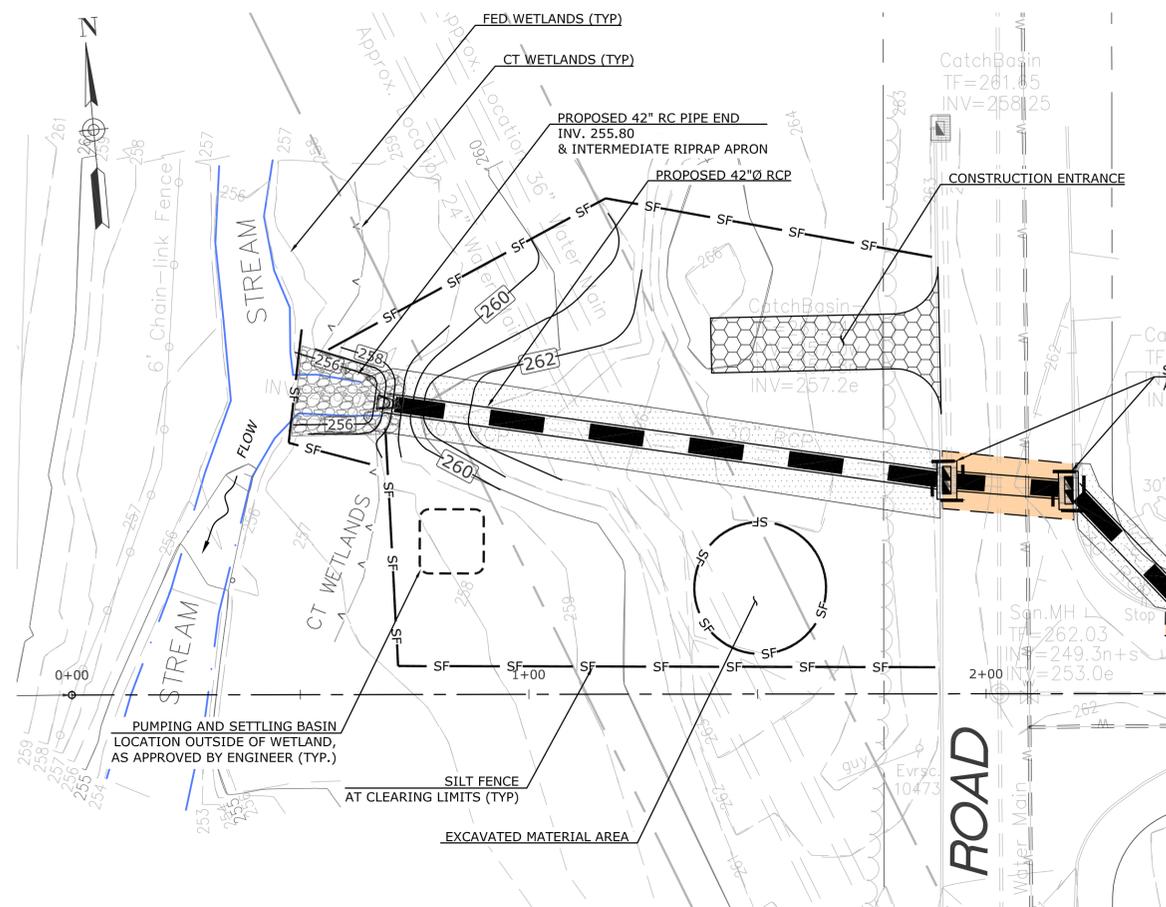
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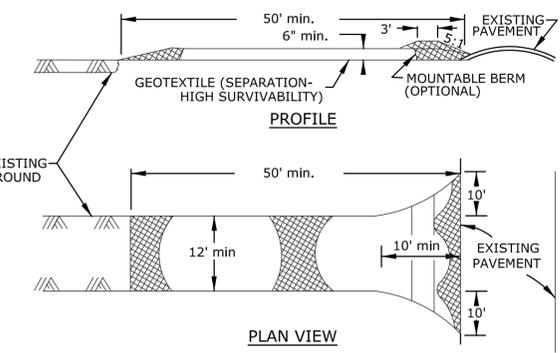
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ROADWAY CONSTRUCTION DETAILS
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT

BRACE AVE	BRACE_FD	22071.10	SHEET	14
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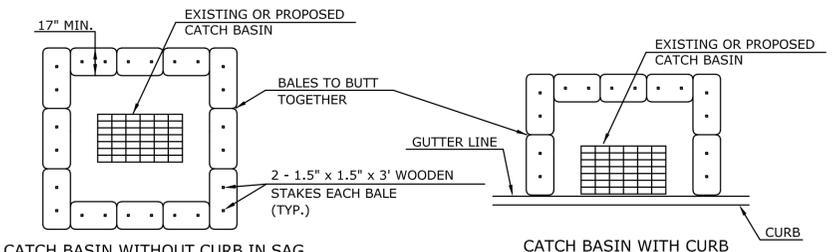


EROSION & SEDIMENTATION CONTROL LAYOUT
SCALE: 1" = 20'-0"



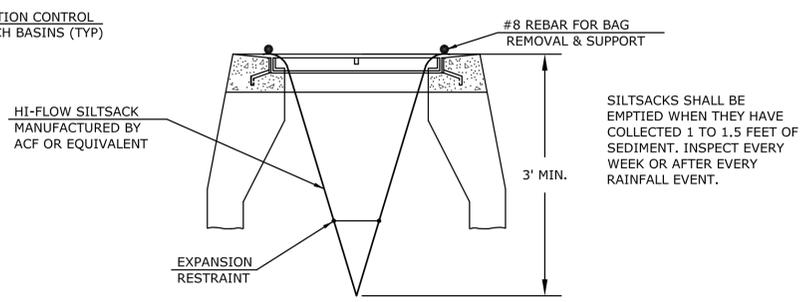
- CONSTRUCTION SPECIFICATION:**
1. STONE SIZE - USE 2" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
 2. LENGTH - AS REQUIRED, BUT NOT LESS THAN 50 FT (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30' MINIMUM LENGTH WOULD APPLY).
 3. THICKNESS - NOT LESS THAN 6".
 4. WIDTH - 12' MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
 5. GEOTEXTILE - WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. GEOTEXTILE WILL NOT BE REQUIRED ON A SINGLE FAMILY RESIDENCE LOT.
 6. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
 7. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHT-OF-WAYS MUST BE REMOVED IMMEDIATELY.
 8. WASHING - WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SETTLING AREA SIZED TO HOLD THE VOLUME OF WATER USED DURING ANY 2-HOUR PERIOD.
 9. PERIODIC INSPECTION AND NECESSARY MAINTENANCE SHALL BE PROVIDED AFTER EACH RAINFALL.

STABILIZED CONSTRUCTION ENTRANCE
N.T.S.

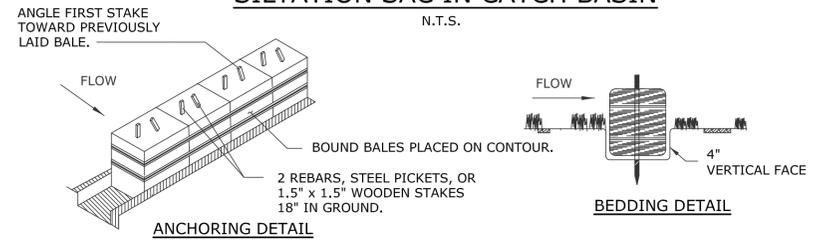


REFER TO PAGE 5-11-33 "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENTATION CONTROL" AND PAGE 40 "ON-SITE MITIGATION FOR CONSTRUCTION ACTIVITIES".

SEDIMENTATION CONTROL AT CATCH BASINS
N.T.S.



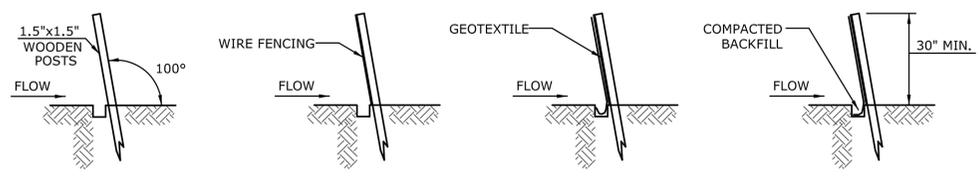
SILTATION SAC IN CATCH BASIN
N.T.S.



- HAY BALE CONSTRUCTION SPECIFICATIONS:**
1. HAY BALES SHALL BE PLACED AROUND NEWLY INSTALLED CATCH BASINS IN SAGS AND DROP INLETS TO PREVENT SEDIMENTATION AND OTHER DEBRIS FROM ACCUMULATING ON THE GRATE OR IN THE SUMP. HAY BALES SHOULD BE KEPT CLEAN AND FREE OF DEBRIS TO FACILITATE FLOW.
 2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4", AND PLACED SO THE BINDINGS ARE HORIZONTAL.
 3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
 4. INSPECTION SHALL BE FREQUENT AND REPAIR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE.

REFER TO PAGE 5-11-30 "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENTATION CONTROL" AND PAGE 53 "ON-SITE MITIGATION FOR CONSTRUCTION ACTIVITIES".

HAY BALE DETAIL
N.T.S.



1. SET POSTS AND EXCAVATE A 6"x6" TRENCH. SET POSTS DOWN SLOPE. ANGLE 10° UPSLOPE FOR STABILITY AND SELF CLEANING.
2. ATTACH THE WIRE MESH FENCING TO POST.
3. ATTACH GEOTEXTILE TO THE WIRE FENCING AND EXTEND IT TO THE TRENCH.
4. BACKFILL THE TRENCH AND COMPACT THE EXCAVATED SOIL.

* WHEN INSTALLATION OF TRENCH IS IMPRACTICAL, ALTERNATE INSTALLATION SHALL BE TO LAY 6" FLAP HORIZONTALLY ON GROUND AND BURY FLAP BY RAMP SOIL OR STONE UP TO CONTROL FENCE. DEPTH OF RAMP SHALL BE AS REQUIRED TO HOLD DOWN FLAP WITHOUT LEAKAGE UNDER CONTROL FENCE WHILE MAINTAINING MINIMUM HEIGHT.

GEOTEXTILE FENCE SYSTEM

REFER TO PAGE 5-11-35 "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENTATION CONTROL" AND PAGE 55 "ON-SITE MITIGATION FOR CONSTRUCTION ACTIVITIES".

SEDIMENTATION CONTROL SYSTEM INSTALLATION
N.T.S.

GENERAL

THIS PLAN PROPOSES EROSION CONTROL MEASURES TO HELP CONTROL ACCELERATED EROSION AND SEDIMENTATION AND REDUCE THE DANGER FROM STORM WATER RUNOFF AT THE SITE. THE RUNOFF SHALL BE CONTROLLED BY THE INTERCEPTION, DIVERSION, AND SAFE DISPOSAL OF PRECIPITATION. RUNOFF SHALL ALSO BE CONTROLLED BY STAGING CONSTRUCTION ACTIVITY AND PRESERVING NATURAL VEGETATION WHENEVER POSSIBLE. EXISTING VEGETATION SHALL BE PROTECTED AND ONLY THAT CLEARING AND GRUBBING ABSOLUTELY NECESSARY FOR THE PROPOSED CONSTRUCTION SHALL BE PERFORMED. ALL DISTURBED AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND CONTOUR, UNLESS OTHERWISE INDICATED ON THE PLANS. THE CONTRACTOR SHALL TAKE SPECIAL CARE WITH HIS CONSTRUCTION METHODS AND SHALL COMPLY WITH THE FOLLOWING GUIDELINES. REFERENCE IS MADE TO THE "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENTATION CONTROL" (2002), AS AMENDED. THE GUIDELINES ARE OBTAINABLE FROM THE CONNECTICUT DEPARTMENT OF ENERGY AND ENVIRONMENTAL PROTECTION, 79 ELM STREET, HARTFORD, CONNECTICUT 06106, AND SHOULD BE USED AS A REFERENCE IN CONSTRUCTING THE EROSION AND SEDIMENTATION CONTROLS INDICATED ON THESE PLANS.

EROSION CONTROL

ALL AREAS SHALL BE PROTECTED FROM EROSION DURING AND AFTER CONSTRUCTION, PARTICULARLY THE STORAGE OF EXCAVATED OR STOCKPILED MATERIAL. THE CONTRACTOR SHALL CAREFULLY STRIP ALL TOPSOIL, LOAM, OR ORGANIC MATTER PRIOR TO TRENCHING OR OTHER OPERATIONS AND SHALL STORE THEM SEPARATELY FROM ALL OTHER MATERIALS DURING EXCAVATION. EACH STOCKPILE MUST BE ADEQUATELY RINGED WITH SEDIMENTATION CONTROL SYSTEM (I.E. HAY BALES AND/OR GEOTEXTILE FENCE). DEBRIS AND OTHER WASTE RESULTING FROM EQUIPMENT MAINTENANCE AND CONSTRUCTION WILL NOT BE DISCARDED ON SITE. STABILIZING OF SLOPES SHALL BE DONE IMMEDIATELY AFTER CONSTRUCTION OF SLOPES. SLOPES STEEPER THAN 3:1 SHALL BE PROTECTED WITH EROSION CONTROL MATTING. THIS MATTING IS MANUFACTURED COMBINATIONS OF MULCH AND NETTING AND SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. ALL OTHER AREAS SHALL BE MULCHED WITH HAY OR STRAW AT A RATE OF 2 TO 3 TONS PER ACRE. STRAW OR HAY MULCH MUST BE ANCHORED IMMEDIATELY AFTER SPREADING TO PREVENT WINDBLOWING. THE METHODS RECOMMENDED BY THE "CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENTATION CONTROL" SHALL BE USED FOR THE ANCHORING OF MULCH OR NETTING.

EROSION AND SEDIMENTATION CONTROL PLAN

AN EROSION AND SEDIMENTATION CONTROL PLAN MUST BE SUBMITTED IN WRITING TO THE ENGINEER AND APPROVED BY THE ENGINEER PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES. SEDIMENTATION CONTROL SYSTEM - THE SEDIMENTATION CONTROL SYSTEM SHALL CONSIST OF A GEOTEXTILE BARRIER FENCE. THE SEDIMENTATION CONTROL SYSTEM SHALL BE INSTALLED IMMEDIATELY AFTER A CUT SLOPE HAS BEEN GRADED, BEFORE A FILL SLOPE HAS BEEN CREATED AND AS INDICATED ON THE PLANS. THE SYSTEM IS DESIGNED TO INTERCEPT SILT AND SEDIMENT BEFORE IT REACHES THE WETLANDS OR WATERCOURSES. DEPOSITS OF SEDIMENT AND SILT ARE TO BE PERIODICALLY REMOVED FROM THE UPSTREAM SIDE OF THE FENCE. THIS MATERIAL IS TO BE SPREAD AND STABILIZED IN AREAS NOT SUBJECT TO EROSION, OR IN AREAS WHICH ARE NOT TO BE PAVED OR BUILT ON. THE SEDIMENTATION CONTROL SYSTEM IS TO BE REPLACED AS NECESSARY TO PROVIDE PROPER FILTERING ACTION. THE SYSTEM IS TO REMAIN IN PLACE AND BE MAINTAINED TO INSURE EFFICIENT SILTATION CONTROL UNTIL ALL AREAS ABOVE THE FENCE ARE STABILIZED AND VEGETATION HAS BEEN ESTABLISHED.

STACKED HAY BALES - HAY OR STRAW BALES USED FOR EROSION CONTROL SHALL BE STACKED AT CATCH BASINS WHERE SEDIMENT MAY ENTER THE CATCH BASIN OR AS DIRECTED BY THE ENGINEER. DEPOSITS OF SEDIMENT AND SILT ARE TO BE PERIODICALLY REMOVED FROM THE UPSTREAM SIDE OF THE EROSION CHECKS. THIS MATERIAL IS TO BE SPREAD AND STABILIZED IN AREAS NOT SUBJECT TO EROSION, OR IN AREAS WHICH ARE NOT TO BE PAVED OR BUILT ON. HAY OR STRAW BALES ARE TO BE REPLACED AS NECESSARY TO PROVIDE PROPER FILTERING ACTION. THE SYSTEM IS TO REMAIN IN PLACE AND BE MAINTAINED TO INSURE EFFICIENT SILTATION CONTROL UNTIL ALL AREAS ABOVE THE EROSION CHECKS ARE STABILIZED AND VEGETATION HAS BEEN ESTABLISHED.

IN ALL AREAS, REMOVAL OF TREES, BUSHES, AND OTHER VEGETATION, AND DISTURBANCE OF THE SOIL, IS TO BE KEPT TO AN ABSOLUTE MINIMUM WHILE ALLOWING PROPER DEVELOPMENT OF THE SITE.

DURING CONSTRUCTION, AS SMALL AN AREA OF SOIL AS POSSIBLE SHOULD BE EXPOSED FOR AS SHORT A TIME AS POSSIBLE. AFTER CONSTRUCTION, GRADE, RESPREAD TOPSOIL, AND STABILIZE SOIL BY SEEDING AND MULCHING AS TO PREVENT EROSION.

EROSION AND SEDIMENTATION CONTROL MAINTENANCE PROCEDURES

ALL EROSION AND SEDIMENTATION CONTROL DEVICES SHALL BE INSPECTED DURING CONSTRUCTION ON A DAILY BASIS AND FOLLOWING ALL STORMS BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN AND MAKE REPAIRS AND REMOVE SEDIMENT AS REQUESTED BY THE ENGINEER. THIS WORK SHALL BE PERFORMED WITHIN 24 HOURS OF THE REQUEST AND THERE SHALL BE NO SEPARATE PAYMENT FOR THIS WORK.

THE CONTRACTOR SHALL CLEAN SEDIMENT AND DEBRIS FROM ALL DRAINAGE STRUCTURES, AND PIPES AT THE COMPLETION OF CONSTRUCTION, AND AS REQUESTED BY THE ENGINEER TO KEEP THE SYSTEM FUNCTIONING PROPERLY DURING CONSTRUCTION.

FOLLOWING COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL REPAIR ALL ERODED AREAS AND ENSURE A GOOD STAND OF TURF IS ESTABLISHED THROUGHOUT. THE CONTRACTOR SHALL REPAIR ALL ERODED OR DISPLACED RIPRAP, AND CLEAN SEDIMENT COVERED STONES.

ALL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES SHOULD BE ESTABLISHED PRIOR TO AND BE MAINTAINED THROUGH ALL CONSTRUCTION PHASES.

WETLAND IMPACTS & DISTURBANCE

EQUIPMENT OPERATING IN WETLANDS: OPERATION OF EQUIPMENT IN WETLAND AREAS IS GENERALLY NOT ALLOWED AND MUST BE APPROVED IN ADVANCE. ANY EQUIPMENT OPERATING IN WETLAND AREAS SHALL BE LOW GROUND PRESSURE (LESS THAN 3 PSI) OR SHALL BE SET ON TEMPORARY FILL OR MATTING. TEMPORARY FILL, TIMBER MATTING OR OTHER MATTING MUST BE APPROVED IN ADVANCE AND WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE GENERAL COST OF OTHER RELATED WORK ITEMS.

TEMPORARY FILL: PLACEMENT OF TEMPORARY FILL (SOIL, RIP RAP, ETC.) IN WETLAND AREAS THAT IS NOT SPECIFICALLY SHOWN ON THE CONTRACT DRAWINGS IS GENERALLY NOT ALLOWED AND MUST BE APPROVED IN ADVANCE. ANY TEMPORARY FILL APPROVED FOR PLACEMENT, SHALL BE PLACED ON GEOTEXTILE LAID ON THE PRE-CONSTRUCTION WETLAND GRADE. UNCONFINED TEMPORARY FILL THAT IS PLACED IN FLOWING WATER SHALL BE ONLY CLEAN WASHED STONE.

WETLAND DISTURBANCE: ONLY THOSE WETLAND AREAS SPECIFICALLY SHOWN ON THE CONTRACT DRAWINGS OR INCLUDED IN APPROVED PERMITS TO BE DISTURBED, OR ADDITIONAL AREAS SPECIFICALLY APPROVED AS ABSOLUTELY NECESSARY TO COMPLETE THE PROPOSED WORK, SHALL BE DISTURBED.

WETLAND & WETLAND FRINGE AREA RESTORATION: ALL DISTURBED WETLAND AND WETLAND FRINGE AREAS SHALL BE RESTORED WITH A WETLAND SEED MIX OR WETLAND TRANSITIONAL SEED MIX CONTAINING ONLY SPECIES NATIVE TO CONNECTICUT. ALL SEED MIX FOR WETLAND OR WETLAND FRINGE (TRANSITIONAL) AREAS MUST BE SUBMITTED AND APPROVED IN ADVANCE. THIS WORK SHALL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE GENERAL COST OF OTHER RELATED WORK ITEMS.

SUPV.	S.R.M.	
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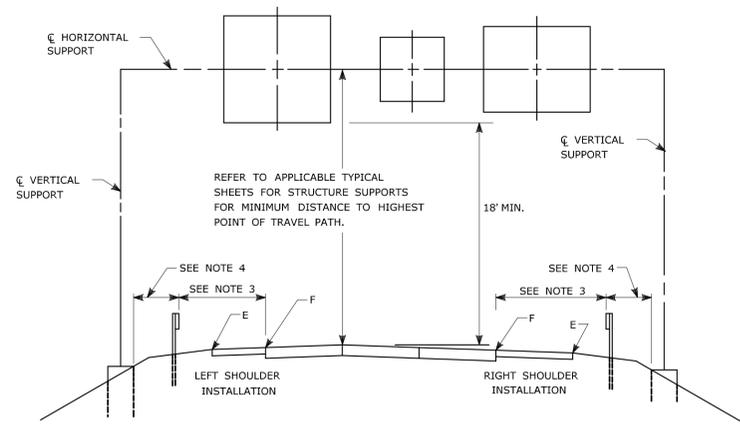
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**EROSION AND SEDIMENTATION CONTROL
BRACE AVENUE DRAINAGE IMPROVEMENTS
BRISTOL, CONNECTICUT**

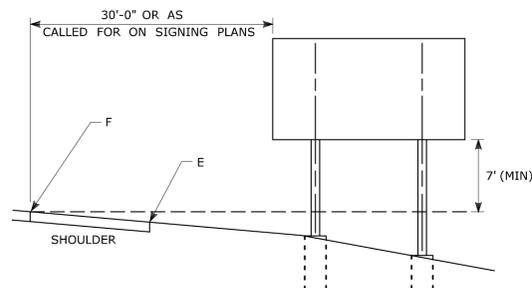
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TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS

NOTES:

- 1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
- 2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANS.
- 3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
- 4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
- 5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.



TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

NOTES:

- 1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
- 2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
- 3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
- 4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.

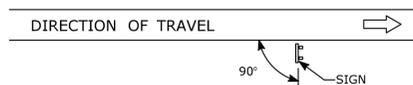


DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

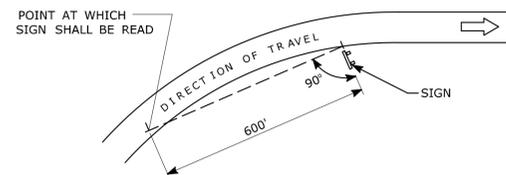


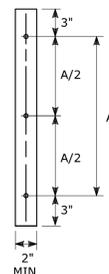
DIAGRAM "B"

SIGN ORIENTATION DETAILS FOR SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

RETROREFLECTIVE STRIPS 48" LONG OR LESS:



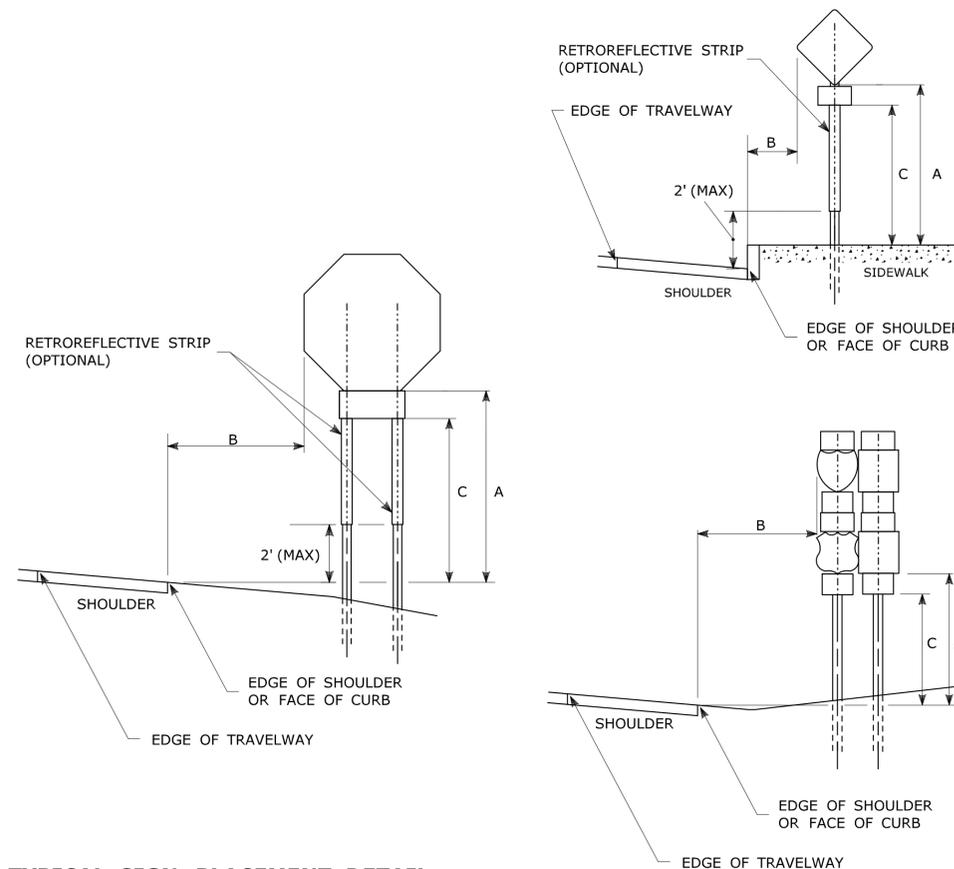
RETROREFLECTIVE STRIPS OVER 48" LONG:



RETROREFLECTIVE STRIP DETAIL

NOTES:

- RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS.
- RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.



TYPICAL SIGN PLACEMENT DETAIL

NOTES:

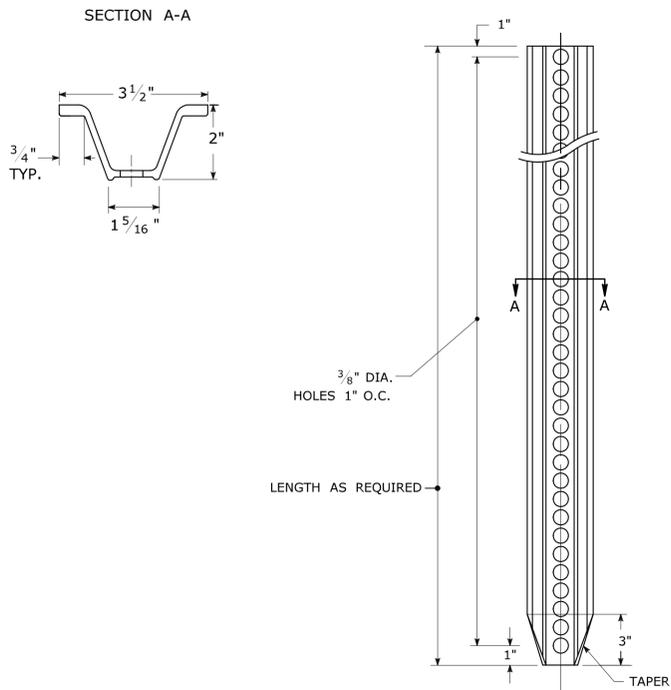
- ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING.
- IF A RETROREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY. PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET (1)	DIM."C" MIN PLAQUE HEIGHT (1)	ASSEMBLY LOCATION
7' (2)	6' (3) 12' (3)	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMPS • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS
4'	6' (3) 12' (3)	N/A	INCIDENT MANAGEMENT SIGNS AND MILE POST MARKER ASSEMBLIES LOCATED ON FREEWAYS AND EXPRESSWAYS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' (4)	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' (4)	7'	SIDEWALKS (5)

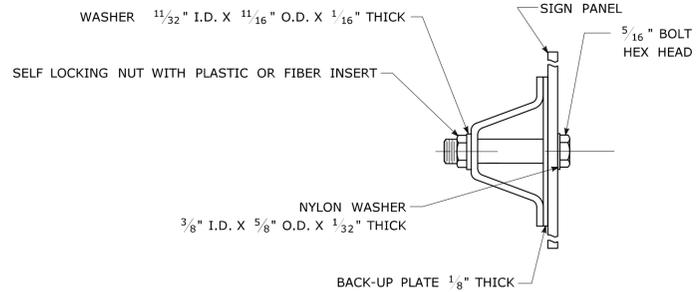
- (1) OR AS DIRECTED BY THE ENGINEER
- (2) 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.
- (3) 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.
- (4) A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.
- (5) A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.

<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p>		<p>NOT TO SCALE</p>		<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>		<p>MARK F. MAKUCH, P.E. 2018.08.17 09:06:06-04'00'</p>		<p>CTDOT STANDARD SHEET</p>		<p>STANDARD SHEET TITLE: SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS</p>		<p>STANDARD SHEET NO.: TR-1208_01</p>	
3	8-2018	INCLUDED INCIDENT MANAGEMENT AND MILE MARKER SIGNS.		<p>Plotted Date: 8/10/2018</p>		<p>MARK F. CARLINO, P.E. 2018.08.21 07:48:06-04'00'</p>		<p>OFFICE OF ENGINEERING</p>					
2	4-2017	MINOR REVISIONS.											
1	2-2011	MINOR REVISIONS.											
REV.	DATE	REVISION DESCRIPTION											

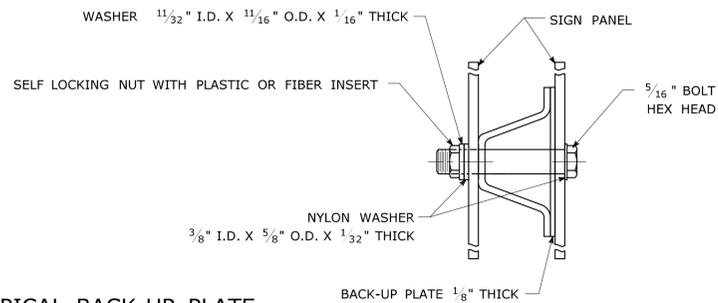
TYPICAL METAL SIGN POSTS



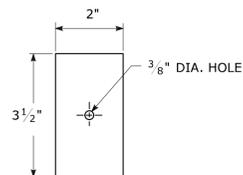
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT



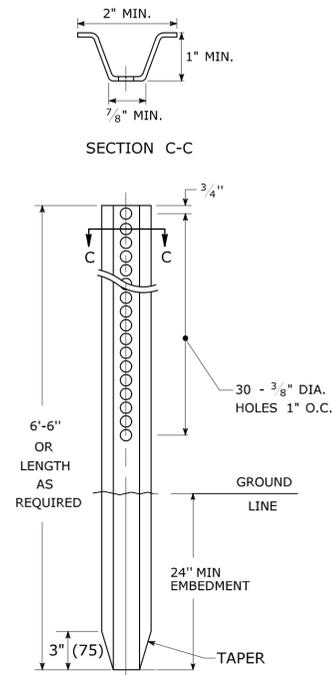
TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
 SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
 WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

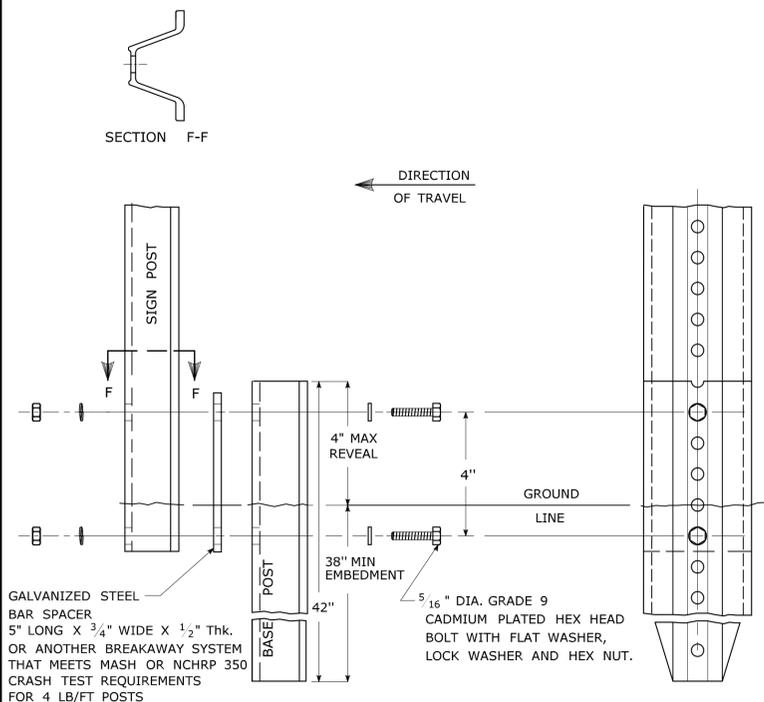
WT./FT. = 1.12 LBS./FT. MIN.



GENERAL NOTES:

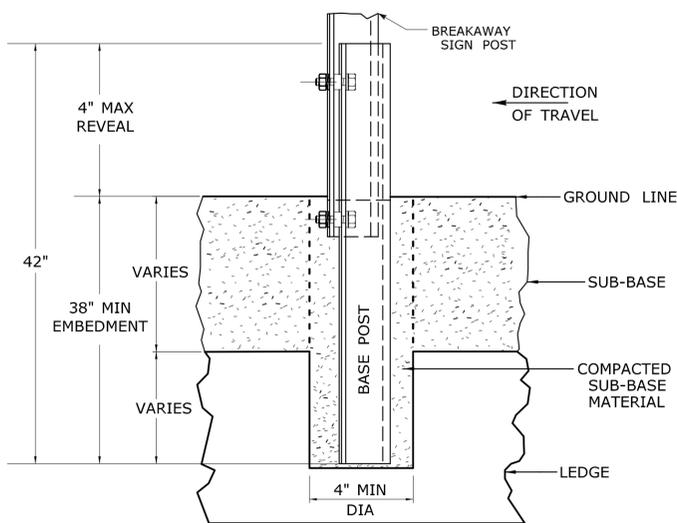
- STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
- AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
- WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
- SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
- ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
- ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 MPH WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- SIGN POSTS SHALL BE 4 LBS./FT.

BREAKAWAY INSTALLATION FOR 4 LBS./FT. POSTS

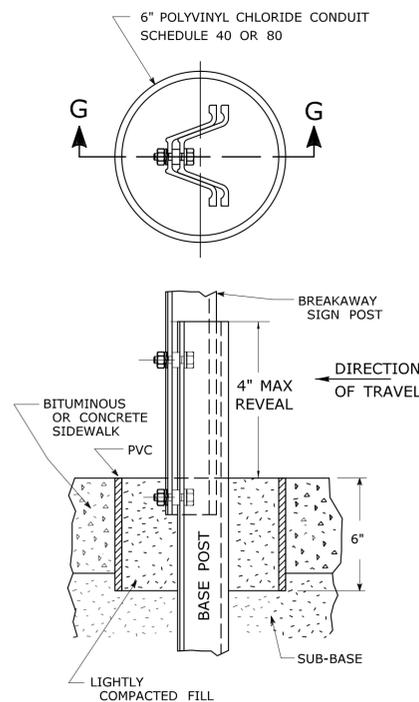


TYPICAL SIGN POST INSTALLATION IN LEDGE

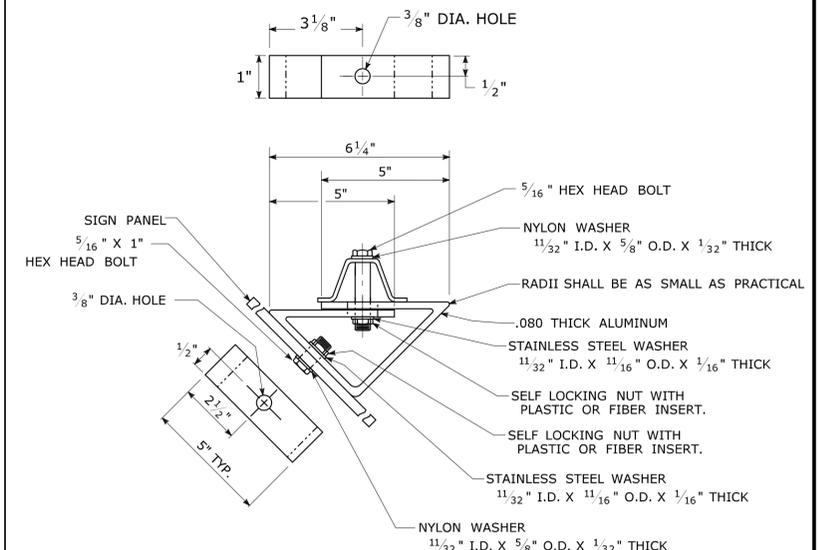
LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".
 HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE FOR PAVED AREAS



45° MOUNTING BRACKET FOR INSTALLATION OF PARKING SIGNS



REV.	DATE	REVISION DESCRIPTION
2	6-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.

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Plotted Date: 6/6/2017

NOT TO SCALE



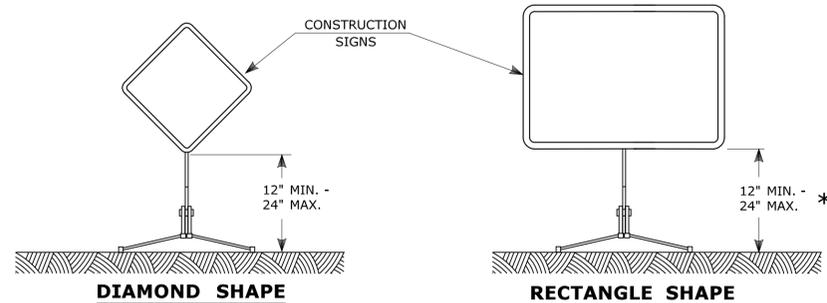
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SUBMITTED BY: *Mark F. Makuch*
 NAME/DATE/TIME: Mark F. Makuch, P.E. 2017.06.07 07:30:30-04'00'
 APPROVED BY: *Mary E. Baker*
 NAME/DATE/TIME: Mary E. Baker, P.E. 2017.06.13 15:28:14-04'00'
Gregory M. Dorosh
 NAME/DATE/TIME: Gregory M. Dorosh, P.E. 2017.06.15 09:27:29-04'00'

CTDOT
 STANDARD SHEET
 OFFICE OF ENGINEERING

STANDARD SHEET TITLE: METAL SIGN POSTS AND SIGN MOUNTING DETAILS
 GUIDE SHEET NO.: TR-1208_02

E5 - SERIES				G20 - SERIES				M4 - SERIES				R1 - SERIES				R9 & R11 - SERIES				W1 - SERIES				W3 - SERIES																																																																																																										
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9.0	36	80-9614	1																																																																																																																															

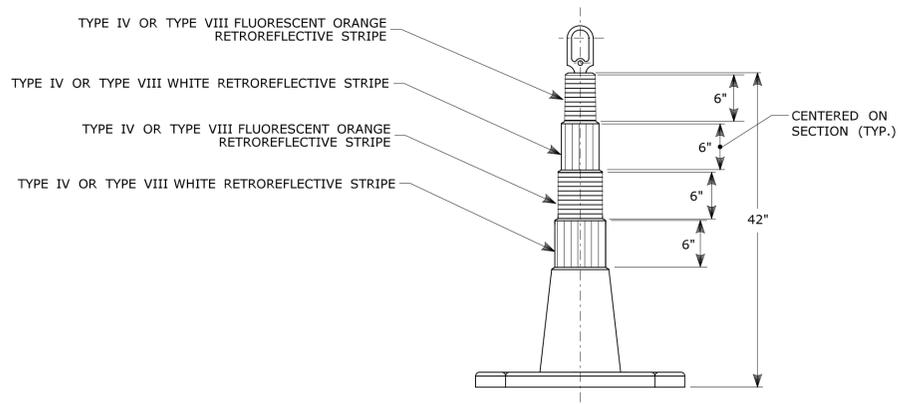


PORTABLE CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220.01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

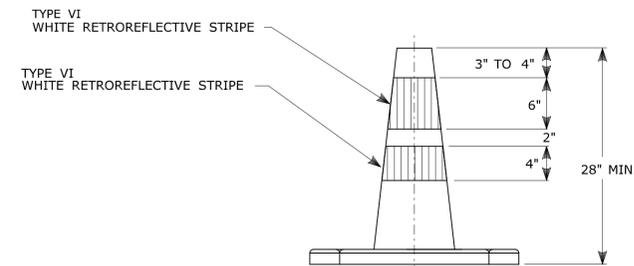
* FOR E5-1 (EXIT SIGNS) USE MIN 48".



42" TRAFFIC CONE

NOTES:

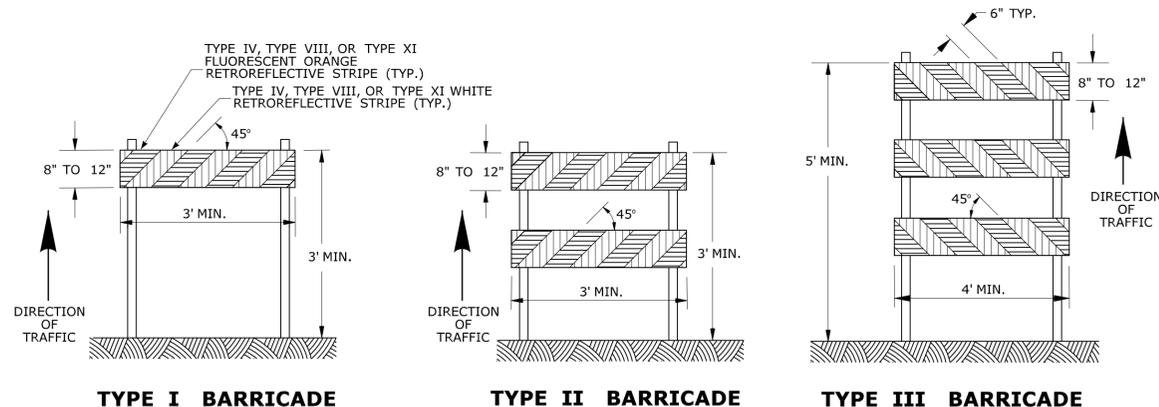
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC CONE

NOTES:

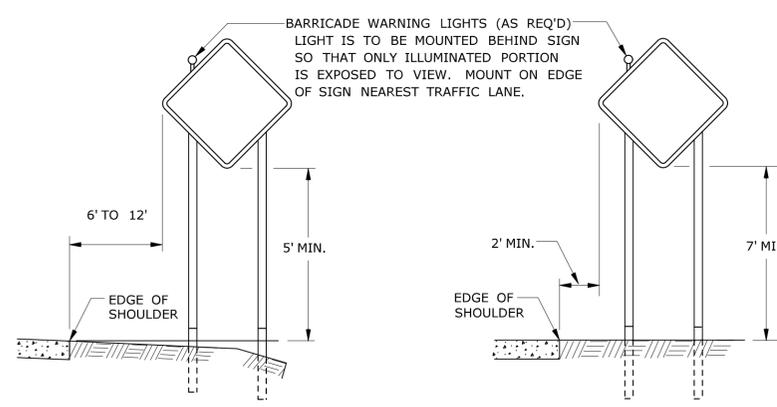
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



CONSTRUCTION BARRICADES

NOTES:

- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE FLUORESCENT ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



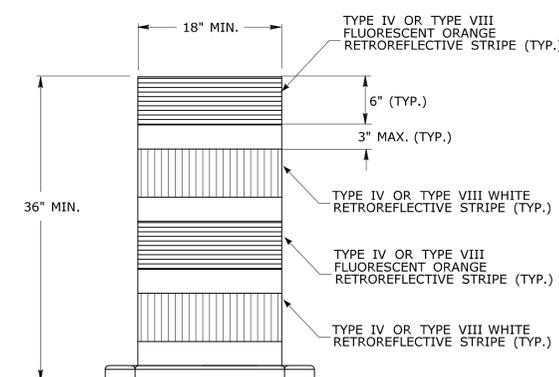
RURAL AREA

URBAN AREA

**PLACEMENT OF CONSTRUCTION SIGNS
TYPICAL LONG TERM INSTALLATION**

NOTES:

- SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.
REFER TO STANDARD SHEETS:
TR-1208.01 - "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS."
TR-1208.02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."



**TRAFFIC DRUM
FRONT VIEW**

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	SUBMITTED BY: <i>Mark Makuch</i> NAME/DATE/TIME: Mark F. Makuch, P.E. 2018.08.17 09:12:43-04'00' APPROVED BY: <i>YFC</i> NAME/DATE/TIME: Mark F. Carlino, P.E. 2018.08.21 07:49:51-04'00'	CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES	STANDARD SHEET NO.: TR-1220_02
3 8-2018 UPDATED SHEETING TYPE AND COLOR. 2 8-2015 UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION. 1 2-2011 MINOR REVISIONS.	NOT TO SCALE Plotted Date: 8/10/2018 Filename: TR-1220_02_3.2018.dgn Model: TR-1220_02					